

(ESTABLISHED 1881.)

宣統元年八月十三日

三拜禮 號大月十英港香

36 ITR ANNUAL
SINGLE COPY, 10 CENTS

Shipping—Steamers

HONGKONG, CANTON, MACAO

AND
WEST RIVER STEAMERS.

JOINT SERVICE OF
THE HONGKONG, CANTON, AND MACAO STEAMBOAT CO., LTD., AND
THE CHINA NAVIGATION COMPANY, LTD.

HONGKONG-CANTON LINE.
S.S. "HONAM" 2,365 Tons, "**FATSHAN**" 2,250 Tons, "**KIINSHAN**" 1,995 Tons.
"HEUNGSHAN" 1,995 Tons.
 Departures from **HONGKONG** to **CANTON** daily at 8 A.M. (Sunday excepted), and 10 P.M. (Saturday excepted).
 Departures from **CANTON** to **HONGKONG** daily at 8 A.M. and 5.15 P.M. (Sunday excepted).
 These Steamers, carrying His Majesty's Mails, are the largest and fastest on the River.
 Special attention is drawn to their Superior Saloon and Cabin Accommodation. Lighted throughout by electricity. Electric Kios in each cabin.

SERVICE OF THE HONGKONG, CANTON AND MACAO STEAMBOAT CO., LTD.

HONGKONG-MACAO LINE.
S.S. "SUI-TAI" 1,285 Tons and "SUI-AN" 1,265 Tons.
Departures from Hongkong to Macao on week days at 8 A.M. and at 1 P.M. from the
Company's Wharf Lok Street Whard.
Departures from Macao to Hongkong on week days at 7.30 A.M. and at 2 P.M.

CANTON-MACAO LINE.
S.S. "HOI SANG," 457 Tons
Departures from MACAO to CANTON on Monday, Wednesday and Friday, at 9 P.M.
Departures from CANTON to MACAO on Tuesday, Thursday and Saturday, at 5 P.M.

JOINT SERVICE OF
HONGKONG, CANTON AND MACAO STEAMBOAT CO., LTD.,
THE CHINA NAVIGATION COMPANY, LTD., AND THE INDO-CHINA STEAM NAVIGATION
COMPANY, LTD.

QANTON-WUCHOW LINE.

S.S. "SAINAM," 558 Tons, and "NANNING," 506 Tons.

One of the above steamers leaves Canton for Wuchow every Monday, Wednesday and Friday, at about 8 A.M., and the other leaves Wuchow for Canton on the same days at 3.30 A.M. Round trips take about 5 days. Passengers can return to Hongkong or Vice Versa by the same passenger direct steamer "Sainam." The vessels are 1st Superior. Cable Accommodation and are lighted throughout by electricity. Electric Fan in each cabin.

EXCURSION TO MACAO.

On **SUNDAY, the 10TH OCTOBER, 1904.**
The Company's Steamship
"SUI-AN"
will depart from the COMPANY'S WING LOK STREET WHARF at 9 A.M.
Departure from Macao at 5 P.M.
Excursion Rates as usual.
N.B.—The Company also runs a steamer from Macao on Sunday morning at 7.30 A.M. and
from Hongkong at 1 P.M. from the Company's Wing Lok Street Wharf. This steamer connects
with the returning steamer from Macao.

Further particulars may be obtained at the Office of the—
HONGKONG, CANTON & MACAO STEAMBOAT CO., LD.,
HOTEL MANSIONS, (FIRST FLOOR),
opposite the Blake Pier.

Hotels.

HONGKONG HOTEL

String Band play during Tiffin and Dinner.

Hongkong, 5th February, 1909.

THE VIENNA CAFE
COMPANY, LIMITED,
No. 34, QUEEN'S ROAD CENTRAL,
Telephone No. 924.

BEG to, notify the Public that A MODERN AND UP-TO-DATE BAKERY AND CAFE
under exclusively European management has been opened at the above entirely rebuilt
and modernized premises.

The latest sanitary improvements employed.
Strictest cleanliness all over the place.
Use only first class flour and other material.

The Company has secured the services of Messrs. J. SOMMER and A. SOKOLOWSKI, former
THE BAKERY AND CONFECTIONERY DEPARTMENT.

The long experience of both Gentlemen in up-to-date establishments on the Continent, is
the best guarantee that only the best ever produced in the Colony will be supplied,
The Patronage of the Public is respectfully solicited.

Hongkong, 20th September, 1909.

(55)

ASTOR HOUSE
(LATE CONNAUGHT HOTEL.)
QUEEN'S ROAD, HONGKONG.

CENTRALLY situated, up-to-date Hotel. Recently renovated, and under entirely New Management. Large and Comfortable Rooms. Excellent Cuisine under the supervision of an Experienced FRENCH CHEF, and separate Tables, Hot and Cold Baths, Electric Light throughout. Terms moderate, First Class accommodation for Families and Tourists.

Under Personal Supervision of
L. GAMEAU,
 Proprietor.
N. BEUMENTHAL,
 Manager.

Telephone, 770, Telegrams "Astor" [44]

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Mails.

NORDDEUTSCHER LLOYD. BREMEN.

IMPERIAL GERMAN MAIL LINES.

FOR	STREAMERS	TO SAIL
SHANGHAI, NAGASAKI, KOBE, and YOKOHAMA	"PRINZ ROY LUITPOLD"..... Capt. H. Kirchner	THURSDAY, 7th Oct., 6 A.M.
MANILA, YAP, NEWGUINEA, BRISBANE, SYDNEY & MELBOURNE	"PRINZ SIGISMUND"..... Capt. D. Lent	FRIDAY, 8th Oct., Daylight.
YOKOHAMA and KOBE	"COBLENTZ"..... Capt. H. Raegener	About SATURDAY, 16th Oct.
KUDAT and SANDAKAN	"BORNEO"..... Capt. F. Samblit	Middle of October.

For further Particulars, apply to

NORDDEUTSCHER LLOYD. MELCHERS & CO., GENERAL AGENTS, HONGKONG & CHINA.

Hongkong, 6th October, 1909.

MESSAGERIES MARITIMES.

FRENCH MAIL LINES.

FORTNIGHTLY SERVICE TO and FROM EUROPE via SUEZ CANAL. TO and FROM JAPAN via SHANGHAI.

FOR	STREAMERS	CAPTAINS	TO SAIL ON
SHANGHAI, KOBE, YOKOHAMA, TOURANE	..... Bourge	.....	11th Oct., P.M.
MARSEILLES, VIA PORTS	..... OCEANIAN	..... Sellier	13th Oct., at 1 P.M.
SHANGHAI, KOBE, YOKOHAMA, ARMAND BEHIC	..... Guibaudet	.....	25th Oct., P.M.
MARSEILLES, VIA PORTS	..... SYDNEY	..... Costa	26th Oct., at 1 P.M.

Transhipment on the Co's Steamers at Singapore for Batavia; at Colombo for Calcutta, Bombay and Australia; at Port Said for the Levant, Constantinople and Black Sea.

Through Tickets to London via Paris from £27.10 up to £71.10. 30 hours' railway from Marseilles to London.

Interpreters meet passengers at their arrival in Marseilles.

For further particulars, apply to

P. de CHAMPMORIN,

AGENT, QUEEN'S BUILDINGS.

Hongkong, 5th October, 1909.

MESSAGERIES CANTONNAISES.

FRENCH LINE OF STEAMERS BETWEEN HONGKONG, CANTON AND KODANG-SI.

S.S. "PAUL BEAU," 1,900 tons, 14 knots.

S.S. "CHARLES HARDOUIN," 1,900 tons, 14 knots.

The speediest, most luxuriously appointed and punctual steamers on the line.

Departure from Hongkong at 4 P.M. (Saturdays excepted).

Departure from Canton at 5.15 P.M. (Sundays excepted).

These superb steamers carrying the French Mail are fitted throughout with Electric Light and Fans and were specially built for this trade. Excellent cuisine.

The Company's Own Wharf, near Wing Lok Street and its berth in Canton opposite Shamen.

For further particulars, please apply to the COMPANY'S OFFICE at Shamen, Canton, or to their Agents

BARRETTO & CO., Hongkong.

Hongkong, 5th October, 1909.

EYES

RIGHT!

N. LAZARUS, OPHTHALMIC OPTICIAN,

CORNER OF D'AGUILAR STREET AND QUEEN'S ROAD.

Will test your eyes free of charge, and if they are wrong will put them right.

Lenses Ground. All kinds of Repairs. Spectacles for all requirements.

Ask, or write, for Illustrated Booklet on "Defective Sight"—free.

LONDON,

BALFOUR,

SHANGHAI,

1, John Street, Bedford Row, W.C.

50, Bealock Street

66, Nanking Road.

Intimations.

THE YOKOHAMA DOCK CO., LTD.

No. 1 DOCK.	No. 2 DOCK.	No. 3 DOCK.
Docking Length 515 ft.	Docking Length 376 ft.	Docking Length 481 ft.
Width of Entrance ... 80 "	Width of Entrance ... 50 "	Width of Entrance ... 63 "
Water on Blocks 28 "	Water on Blocks ... 26 "	Water on Blocks 21.5 "

THESE DOCKS are conveniently situated in Yokohama harbour and the attention of Captains and Engineers is respectfully called to the advantages offered for Docking and repairing Vessels and Machinery of every description.

The plant and tools are of recent patterns for dealing quickly and cheaply with work and a large stock of material is always at hand, (plates and angles all being tested by Lloyd's surveyors).

Two powerful Twin Screw Tugboats are available for taking Vessels in or out of Dock, and for taking Sailing Vessels in or out of the bay. The floating derrick is capable of lifting 40 tons.

Steam Launches of Steel or Wood, Lighters, Steel Buildings and Roofs, Bridge Work, and all kinds of Machinery are made on the premises.

Tenders will be made up when required and the workmanship and material will be guaranteed.

The cost of Docking, and repair work, will be found to compare favourably with that of any port in the world.

A large mooring basin is available alongside our own works for mooring vessels whilst under repairs.

Telephone: Nos. 876, 506, or 681.

Telegrams, "Dock, Yokohama," Codes A. B. C. 4th and 5th Edt.

Liebers, Bootts, A. I. and Watkins.

Yokohama, April 28th, 1903

F. BLACKHEAD & Co., SHIP-CHANDLERS, SAILMAKERS, COAL AND PROVISION MERCHANTS, NAVAL CONTRACTORS AND GENERAL COMMISSION AGENTS. GARDEN FLOOR, ST. GEORGE'S BUILDING, HONGKONG. SOAP AND SODA MANUFACTURERS.

SOLE AGENTS FOR HARTMANN'S RAHTIEN'S GENUINE COMPOSITION RED HAND BRAND. HARTMANN'S GREY PAINT. DAIN'S PATENT MOTOR LAUNCHES.

SOLE AGENTS FOR FERGUSON'S SPECIAL CREAM and P. & O. SPECIAL LIQUOR BOOTHS WHISKY, &c.

EVERY KIND OF SHIP'S STORES AND REQUISITES ALWAYS IN STOCK AT REASONABLE PRICES.

OSMAN & CASUM, 1 & 3, D'AGUILAR STREET.

JUST UNPACKED Ladies' Trimmed and Untrimmed HATS, RIBBONS, FLOWERS & FEATHERS.

MUSLIN and FIGURED VOILES.

LACE and EMBROIDERIES a speciality.

TABLE LINENS, SERVIETTES and HOUSEHOLD LINENS.

Samples on application.

Coast Port Orders carefully executed

Hongkong, 5th September, 1909

D. NOMA, PROFESSIONAL TATTOOER and THE EXPERT REMOVER OF TATTOO MARKS. No. 60, QUEEN'S ROAD, CENTRAL.

PATRONISED by Prince of Wales, then H. R. H. The Emperor of York, and H. R. H. The Emperor of Russia, and having 4,500 testimonials from all sources.

My 34 years' experience in tattooing is a guarantee of good work and prompt execution. My colours are absolutely fast and perfectly harmless, and produce a charming effect not attained by any other, as their composition is only known to me. In tattooing unlike some species of engraving, care must be taken to have the work done in a perfect, high toned manner. In order to take special precaution against possible danger, I use fresh materials daily.

The copying of Portraits with distinct minutiae a speciality.

Hongkong, 1st September, 1908

To Let.

TO LET. IN No. 6, DES VŒUX ROAD CENTRAL, Offices and Godown. In No. 5, QUEEN'S ROAD CENTRAL, Victoria Building, Rooms suitable for Offices. ROOMS in College Chambers, No. 31, WYNDHAM STREET. Apply to— DAVID SASSOON & Co., Ltd. Hongkong, 15th September, 1909.

TO LET. 158, PRAYA EAST. Apply to— JARDINE, MATHESON & Co., LTD. Hongkong, 12nd September, 1909.

TO LET IN CANTON FROM 1ST PROX. TWO SEMI-DETACHED HOUSES each containing Eight Rooms with Back Yards, and Servants' Quarters on Shamen Lot 15, now in the occupation of the Mitsui Bussan Kaisha. Apply to— DAVID SASSOON & Co., Ltd. Hongkong, 4th October, 1909.

TO LET. KING'S BUILDINGS, OFFICES facing the Harbour from about October, at present in occupation of Messrs. Jardine, Matheson & Co., Ltd. Apply— THE HONGKONG LAND INVESTMENT & AGENCY CO., LTD. Hongkong, 3rd June, 1909.

TO LET. OFFICES and ROOMS on the 2nd Floor of No. 14, Des Vœux Road Central (formerly occupied by Messrs. Shaw, Thomas & Co.). Apply to— THE COMPTON DEPARTMENT, E. D. Sassoon & Co., Queen's Road Central, Hongkong 11th September, 1909.

TO LET. No. 1 & 3 MORRISON HILL, also OFFICES at No. 1 PRUDER STREET. Apply to— Messrs. JARDINE, MATHESON & Co., Ltd. Hongkong, 19th May, 1909.

TO LET. OFFICES, No. 2, CONNAUGHT ROAD, 3rd Floor. No. 3, CLIFTON GARDENS, CONDUIT ROAD. A HOUSE in WONG-MEI-CHONG ROAD. A HOUSE in RISON TERRACE. GODOWNS in PRAYA EAST, BLUE BUILDINGS, and No. 160, Des Vœux Road next to the Hongkong Hotel. FLATS in MARKET TERRACE, No. 10, DES Vœux Road CENTRAL, 1st Floor. Apply to— THE HONGKONG LAND INVESTMENT & AGENCY CO., LTD. Hongkong, 1st June, 1909.

TO LET. GODOWN, No. 1A, DUDDELL STREET. Apply to— THE HONGKONG LAND INVESTMENT & AGENCY CO., LTD. Hongkong, 1st June, 1909.

JAPAN'S FINANCIAL POSITION.

STATEMENTS BY THE GOVERNMENT.

On the morning of the 22nd ultimo, Messrs. Mori and Gode, members of the Political Affairs Investigation Committee of the Progressist party in the Diet, called at the Finance Department and interviewed the Directors of the Revenue Accountants and National Debt Consolidation Bureaux. They sought the views of the Government on various financial questions, and the following summary will give an idea of the information gleaned:—

READJUSTMENT OF TAXATION

The plan for the readjustment of taxation is based on the two-fold policy of making the burden of taxes on the people equitable and at the same time of reducing that burden to some extent. The taxes which could be reduced have been carefully investigated, but so far the policy of readjustment of taxation only has been decided on, and the actual taxes to be reduced have not yet been indicated by the Cabinet.

THE SUGAR EXCISE QUESTION.

It is a mistake to suppose that a bounty will be granted to the sugar industry in Formosa in compensation for the strict enforcement of the Sugar Tax law in the island. These are wholly independent questions. The granting of a bounty is the business of the Formosan Government, and the Finance Department was duly consulted on the matter. The scheme will be included in the Budget for the Formosan Government, and submitted to the Diet in the coming session. The Finance Department is strictly enforcing the Sugar Tax law, and has thoroughly investigated the question as to whether sugar should be sub-divided into more classes than four as at present for purposes of taxation. The Government is not yet in a position to say precisely what steps will be taken in the matter.

THE DAI NIPPON SUGAR COMPANY.

The law provides that the collection of Sugar Tax may be delayed for six months if proper security is deposited. The legal term of grace allowed in the case of the Dai Nippon Sugar Company has already expired, but the collection of the tax is still delayed, because, if enforced, it would involve the complete collapse of the company, and would lead to serious financial complications. An application has been filed by the company requesting the privilege of being allowed to pay off the tax in instalments, but the authorities have not yet had time to consider this application.

EVASION OF SALT TAX.

The illicit brewing of salt is carried on in the North-East while the evasion of salt-tax is most common in the South-West. The District Revenue Inspection Offices have been given strict instructions to exercise the utmost care in selecting Revenue officers in order to avoid corruption arising out of connivance between the officials and the salt brewers. Measures are being enforced to put a stop to such practices by appointing special inspectors, or by invoking the co-operation of the police. In addition to this, the duty of every citizen to pay the lawful duties and taxes is being taught in the schools with the object of giving the young a proper knowledge of their obligations in later years. Lecturers are also engaged to give addresses on this subject, illustrated by lime-light pictures.

SURPLUS OF REVENUE.

A detailed statement of accounts relating to the surplus revenue for the 41st fiscal year (1906-7) will be published before the end of November next. The accusation has been made that the accounts of the surplus revenue published by the Government in the past have been inaccurate, and it has even been alleged that such inaccuracy was intentional. The accounts may have appeared to be inaccurate owing to the difficulty of drawing them up but in future more concise and accurate accounts will be published.

INTEREST CHARGED BY SPECIAL BANKS.

The maximum rate of interest charged on advances made by the Noko Ginko (Agricultural and Industrial Banks conducted with certain special privileges granted by the Government) is fixed by the Finance Department every six months, the decision of the Department being based on the condition of the money-market in Japan. The rates now charged, in the opinion of the Department, are not excessively high. The allegation that the rate charged by the Formosan and the Hokkaido Colonial Banks is excessively high will be investigated by the authorities.

THE ORIENTAL DEVELOPMENT COMPANY.

The president and all other officials of the Oriental Development Company are making every effort to discharge their duties fully and satisfactorily. They are now surveying the land handed over by the Korean Government in consideration of its shares in the company, amounting to ¥3,000,000 and the economic relations of agricultural industry in all parts of the peninsula are being investigated. The company is now endeavouring to cultivate vegetables and fruit on the farm lands in the vicinity of Seoul, and is also seeking to obtain fishing rights from the Korean Government. This is with the object of establishing colonies of fishermen, as set forth in the Articles of Association of the company, not for the purpose of disposing of these rights at a profit, as has been alleged. In Korea there are now six agricultural and industrial banks and a number of other financial corporations, but their aggregate capital is not more than ¥100,000,000, a sum which is quite inadequate to effect the desired industrial development of Korea. This has prompted the Government to amend the Articles of Association of the company and to open the business of advancing money for the encouragement of industries on the security of immovable property and open shares.

WEATHER-FORCAST AND STORM-WARNINGS ISSUED FROM THE HONGKONG OBSERVATORY.

METEOROLOGICAL SIGNALS.

Meteorological signals are hoisted on the mast in front of the Water Police Station at Tsim Sha Tsui for the information of masters of vessels leaving the port. They do not necessarily imply that bad weather is expected here:—

1. A CONE point upwards indicates a Typhoon to the North of the Colony.
2. A CONE point upwards and DRUM below indicates a Typhoon to the North-East of the Colony.
3. A DRUM indicates a Typhoon to the East of the Colony.
4. A CONE point downwards and DRUM below indicates a Typhoon to the South-East of the Colony.
5. A CONE point downwards indicates a Typhoon to the South of the Colony.
6. A CONE point downwards and BALL below indicates a Typhoon to the South-West of the Colony.
7. A BALL indicates a Typhoon to the West of the Colony.
8. A CONE point upwards and BALL below indicates a Typhoon to the North-West of the Colony.

Red Signals indicate that the centre is believed to be more than 300 miles away from the Colony.

Black Signals indicate that the centre is believed to be less than 300 miles away from the Colony.

The above signals will, as heretofore, be hoisted only when typhoons exist in such positions or are moving in such directions that information regarding them is considered to be of importance to the Colony or to shipping leaving the harbour.

These signals are repeated at the Harbour Office, H.M.S. Tamar, Green Island Signal Mast, and the Flagstaff on the premises of the Hongkong and Kowloon Wharf and Godown Company at Kowloon.

URGENT SIGNAL.

In addition to the above, when it is expected that the wind may increase to full typhoon force at any moment, the following Urgent Signal will be made at the Water Police Station, and repeated at the Harbour Office:—

THREE EXPLOSIVE BOMBS, AT INTERVALS OF TEN SECONDS.

A Black Cross will be hoisted at the same time, superior to the other shapes.

NIGHT SIGNALS.

The following Night Signals will be exhibited from the Flagstaff on the roof of the Water Police Station at Kowloon, the Harbour Office Flagstaff, and H.M.S. Tamar.

I. Three Lights Vertical, Green Green Green, indicates that a typhoon is believed to be situated more than 300 miles from the Colony.

II. Three Lights Vertical, Green Red Green, indicates that a typhoon is believed to be situated less than 300 miles from the Colony.

III. Three Lights Vertical, Red Green Red, indicates that the wind may be expected to increase to full typhoon force at any moment.

No. III. Signal will be accompanied by the Explosive Bombs, as above, in the event of the information conveyed by this signal being published by night.

These Night Signals will be exhibited the Day Signals at sunset, and will, when necessary, be altered during the night.

SUPPLEMENTARY WARNINGS.

For the benefit of Native Craft and passing Ocean Vessels, a Cone will be exhibited at each of the following stations during the time that any of the above Day Signals are hoisted in the Harbour.

Gap Rock.	Aberdeen.
Waglan.	Sau Ki Wan.
Stanley.	Sai Kung.
Cape Collinson.	Sau Tau Koi.
	Tai Po.

This will indicate that there is a depression somewhere in the China Sea, and that a Storm Warning is hoisted in the Harbour.

Further details can always be given to Ocean Vessels, on demand, by signal, from the "Night" houses.

F. G. Fison, Director.

Intimation.

Powell's

HAVE JUST
OPENED THE
FIRST
CONSIGNMENT
OF

Ladies'
AUTUMN

and
WINTER

COATS,
SKIRTS,
COSTUMES,
ULSTERS

ALSO
A good variety of
CHILDREN'S
COATS.

The cut of these Gar-
ments is Superb and the
newest colorings only
are displayed.

SHOW ROOMS
AND
FITTING ROOM
ALEXANDRA
BUILDINGS.

Hongkong, 5th October, 1909.

Hongkong, 5th October, 1909.

Hongkong, 5th October, 1909.

Public Company

CANTON INSURANCE OFFICE,
LIMITED.

NOTICE TO SHAREHOLDERS.

THE TWENTY-EIGHTH ORDINARY
MEETING OF SHAREHOLDERS will
be held at the Office of the Under-
signed on FRIDAY, the 22nd inst., at 11 A.M.
The TRANSFER BOOKS of the Company
will be CLOSED from the 8th to the 22nd
instant, both days inclusive.

JARDINE, MATHESON & Co., Ltd.,
General Agents.
Hongkong, 5th October, 1909. [702]

Notice of Firm.

HONGKONG AND WHAMPOA DOCK
COMPANY, LIMITED.

NOTICE.

NOTICE is hereby given that Mr. R. M.
DYER, having been appointed CHIEF
MANAGER of the Company, takes up the
duties of the position from this date.

By Order of the Board of Directors,
W. J. GRES-ON,
Chairman.
Hongkong, 5th October, 1909. [696]

Auctions.

PUBLIC AUCTION.

THE Undersigned will sell by PUBLIC
AUCTION,
TO-MORROW (THURSDAY),
the 7th October, 1909, at 2.30 P.M., at his Sales
Rooms, Duddell Street,

A QUANTITY OF
HIGH CLASS IRISH GOODS,
Comprising:—

TABLE LINENS, SERVIETTES,
HOUSEHOLD LINEN, HUCKABACK
TOWELS, TURKISH TOWELS, BATH
SHEETS, BROWN LINEN TOWELS,
LADIES' WHITE LAWN UNDERSKIRTS,
WALKING SKIRTS, COMBINATIONS,
ROBES, FLANNELLETTES, NIGHT
DRESSES, DRESSING GOWNS, LA-
DIES' DRESS LENGTHS;

ALSO

An Assortment of SUIT LENGTHS,
LACE CURTAINS and WITNEY BLAN-
KETS, &c., &c.

(All New Goods).

Catalogues will be issued.

On View on Wednesday, the 6th October.

Terms:—As usual.

G. P. LAMMERT,
Auctioneer.
Hongkong, 30th September, 1909. [688]

PARTICULARS AND CONDITIONS of
the letting by Public Auction Sale, to
be held on MONDAY, the 11th day of October,
1909, at 3 P.M., at the Office of the Public
Works Department, by Order of His Excellency
the Governor, of One Lot of CROWN LAND
near Shaikwan, Island Lot No. 114, in the
Colony of Hongkong, for a term of 75 years,
with the option of renewal at a CROWN
RENT to be fixed by the Surveyor of His
Majesty the KING, for one further term of 75
years.

PARTICULARS OF THE LOT.

No. of Sale.	Boundary Measurements.	Contents in Sq. Ft.	Annual Rent.	Upset Price.
Shaukwan Island Lot No. 114	75 75 65 60	4,500	4	4,500
Shaukwan Island Lot No. 114	75 75 65 60	4,500	4	4,500

Hongkong, 5th October, 1909. [699]

Consignees.

NOTICE TO CONSIGNEES.

THE P. & O. S. N. Co.'s Steamer
"SOMALI,"
FROM ANTWERP, LONDON, MALTA,
PORT SAID, SUEZ AND STRAITS.

Consignees of Cargo by the above-named
vessel are hereby informed that their Goods are
being landed and placed at their risk in the
Hongkong and Kowloon Wharf and Godown
Company's Godowns at Kowloon, where each
consignment will be sorted out mark by mark,
and delivery can be obtained as soon as the
Goods are landed.

Optional Goods will be landed here unless
instructions are given to the contrary before
6 hours.

Goods not cleared by the 30th instant, at
4 P.M., will be subject to rent.

No Fire Insurance will be effected by me in
any case whatever.

Damaged Packages must be left in the
Godowns for examination by the Consignee's
and the Company's representative at an
appointed hour.

All claims must be presented within ten
days of the steamer's arrival here after which
date they cannot be recognised.

No claims will be admitted after the goods
have left the Godowns.

E. A. HEWETT,
Superintendent.
Hongkong, 24th September, 1909. [714]

NOTICE TO CONSIGNEES.

STEAMSHIP "VINE BRANCH."

FROM SYDNEY & NEWCASTLE, N.S.W.

THE above Steamer having arrived, Con-
signees of Cargo are hereby requested to
send in their Bills of Lading for countersig-
nature and to take immediate delivery of their
Goods from alongside.

Cargo impeding the discharge of the Vessel
will be landed and stored at Consignee's risk
and expense.

No Fire Insurance will be effected by us in
any case whatever.

DODWELL & CO., LIMITED,
Agents.
Hongkong, 2nd October, 1909. [693]

LADY SASSOON'S WILL.

LARGE BEQUESTS OF JEWELLERY.

Dame Alice Caroline Sassoon, of Park-lane,
W., and Avenue Marigny, Paris, who died on
July 28 last, wife of Sir Edward Sassoon, M.P.
and daughter of Baron Gustave de Rothschild,
president of the Butler-street Jewish Girls'
Club, left estate valued at £240,972 gross, of
which £318,972 is net personally.

Probate of her will, with two codicils, has
just been granted in London to her husband,
Sir Edward Ezra Sassoon, Bart., of Leaden-
hall-street, E.C., and Baron Léon Lambert, of
Brussels.

The testatrix left to her son Philip all her
sapphires and all her solitaire diamond stones,
diamond ring, her long chain of small diamonds,
and the two longer rows only of her three-row
pearl necklace, a big diamond bow, her diadem,
and all her diamonds.

The other row of pearls from her necklace
she left to her daughter.

To her sister, Lucie Lambert, a bracelet set
with black and white pearls.

To her brother-in-law, Léon Lambert, a ring
with a square emerald.

To Henri Lambert, a ring with a square
sapphire.

To Mme. Marthe Emden a pendant with
emeralds.

To her sister-in-law, Nelly de Rothschild,
one of the bracelets with alternate pearls and
another such bracelet each to René Lambert
and Betty Lambert.

To Claude Stern, a bracelet with coloured
stone and diamond.

To her eldest son, the large pearl necklace
presented to her by her husband on their
marriage.

To her other sons, in shares as nearly equal
in value as may be and failing such other
sons, then to her daughters) her round diamond
brooch with pearl centre and pear-shaped pearl
drop, her diamond bow with pearl drop, two
pearl-shaped emerald pins, and two round
diamond bracelets.

To her niece, Antoinette Leonino, a diamond
bow brooch which belonged to her mother.

To her niece, Marquise Leonino, a gold
bangle with two white pearls, which belonged
to her mother.

And to each of her nephews and nieces a
bracelet or brooch to be selected for him or her
by her executors.

To her brother Robert, as a souvenir, the
pink pearl with the diamonds surrounding it,
forming the clasp of her pearl necklace, and
some other article, as selected by her husband.

One of her brooches, as her husband and
her sister, Lucie Lambert, may select, each to
Rachel Reuben Sassoon, Louise Arthur Sas-
soon, and Mireille Hyem.

A memento, as her husband and her said
sister may select, to her brother-in-law, Em-
manuel Leonino, and each of her intimate
friends.

The balance of her jewellery and all furs and
laces to her daughters.

Funds, the subject of marriage settlement,
dated October 19, 1887, and subject to deed of
partial resettlement, dated September 21, 1892,
of which she had power of appointment, are
subject to her husband's life interest, to be di-
vided amongst her children in equal shares.

To each of her children attaining majority an
annuity during the life of her husband of £2,400.

The residue of her estate she left to her hus-
band for life, with remainder to her children in
equal shares.

SECRET ROYAL MATCH.

WEDDING OF PRINCE WHO GAVE UP RIGHTS

It became known yesterday, 3rd Sept., that
Prince Herman of Saxe-Weimar, a member of
the reigning family of that grand duchy, who
recently renounced his claims to the suc-
cession, had been married quietly in London the
previous day to a German lady.

This young lady, the daughter of a captain in
the German merchant-service, has been stay-
ing, under the title of Countess Neustadt, at
the Hyde-park Hotel, where the prince has also
stayed under the name of Count Herman
Charles Ostheim.

The wedding took place in the quietest pos-
sible manner at the office of the Registrar of
St. George's, Hanover-st., in Prince's-row,
Buckingham Palace-yard. It was by special
license.

ARRIVED IN TAXI-CAB.

The couple arrived at the office in an ordi-
nary taxi-cab, accompanied by two other
persons—the count's solicitor and his clerk—as
witnesses, and there was nothing to indicate
that any other but the most ordinary marriage
was taking place.

The count's entry was:
Herman Charles Ostheim, 23, count of the
German Empire, son of William Saxe-
Weimar, Highness and Prince of Saxe-
Weimar.

The bride's entry was:
Wanda Paola Lottero, 25, daughter of Jean
Baptiste Lottero (deceased), captain in mer-
chant service.

The two witnesses were Mr. A. St. George
Cauffield and Thomas K. Cockington.

Prince Herman of Saxe-Weimar is the eldest
son of Prince William of Saxe-Weimar, and
nephew of the reigning Grand Duke. The
family are related to his Majesty, for the late
Prince Edward of Saxe-Weimar, who died in
1902, was a cousin of Queen Victoria, and a
nephew of William the Fourth.

A SELF-WILLED LINE.

The Weimar princes have been famous for
the wilfulness of their youth. Prince Herman's
father (Prince William) was sent to America in
consequence of the prodigality of his young
days, and in the United States is said to have
followed many occupations hardly in keeping
with the position of a prince before he returned
to Germany, having sown his wild oats.

His uncle, Prince Bernhard of Saxe-Weimar,
in 1901, secretly married the Countess Succe-
sini, who was the daughter of an innkeeper in
Lubeck, and a widow. As the duke refused to
sanction the marriage, Prince Bernhard re-
nounced his rank and title, and adopted that
of Count de Crayenberg.

Another of the Saxe-Weimar family, the
Grand Duchess Pauline, created a great sensa-
tion some years ago by contracting a mar-
riage with her chamberlain.

AGROUND IN SHIP CHANNEL.

JAPANESE STEAMER TAKES WRONG COURSE.

The *Shanghai Times*, of 30th ult., reports:—
The *Chishima Maru* at about eleven o'clock
on Tuesday evening ran ashore on the stone
crib which extends from the works at the upper
end of Gough Island to the dolphin pile. The
vessel ought never to have got into Ship
Channel, having in view the fact that the
"Astraea" Channel has been open for some
time now and notices of its opening were sent
all over the East. In addition the guiding
lights have been removed from Ship Chan-
nel. However, it seemed that the *Chishima
Maru*, with a load of railway sleepers,
entered Ship Channel and steamed along in the
dark. It was not until the steamer, which has
a net tonnage of 877, had almost traversed the
entire channel that she struck bow on to the
stone crib, which was covered with water,
the tide being almost full high. It was found
impossible to get the steamer off during the
night, consequently the work of unloading was
commenced and lugs were sent for. Yesterday
the work was continued and the lugs held in
readiness to pull her off. During the day it
was reported that she sprung a leak and fears
are entertained for her safety. The steamer
is owned by the Mitsui Bussan Kaisha.

STEAMER RE-FLOATED.

October 1.
The efforts made on Wednesday night to
get the Japanese steamer *Chishima Maru*
off the stone crib in Ship Channel were
successful, thanks to the assistance rendered
by the Kochian Transportation and Tow
Boat Company's tug *Shun Fung*, com-
manded by Captain A. V. Brown. Fortu-
nately the tide at the present time
are high, which facilitated the work of
salvage, and when the portion of the *Chishima
Maru's* cargo was unloaded the tug was able to
pull her off and tow her into deep water. A
curious examination revealed no serious
damage but it is anticipated that many of her
plates must be sprung. It was also ascertained
that the steamer missed the open channel by
about only ten feet. The *Chishima Maru* will
be brought up to Shanghai shortly.

COTTON YARN.

THE MARKET IN JAPAN.

The price of raw cotton has been rapidly
forced up since the beginning of this month,
when the failure of the cotton crop in America
was reported by the U.S. Agricultural Bureau.
The *Osaka Mainichi* remarks that the question
now arises whether the prevailing high market
will continue much longer. If the advance is
due, not to a temporary and artificial cause,
but to an actual shortage in the supply, the
market will not decline, and it is only too evi-
dent that the cotton-spinning industry in Japan
will be the worst sufferer. Compared with
this time last year, the prevailing price of
raw cotton is over ¥30 higher, but the ad-
vance in the price of cotton yarn is not
more than ¥20. The difference of ¥10 be-
tween the increased price of raw cotton
on the one hand and of yarn on the other is a
severe blow to spinners, but they will be more
immediately affected by the small stock of raw
cotton held by them and the restricted con-
tracts made by them before the rise in price.
The smaller spinning companies, owing to their
financial position, cannot hold large supplies of
cotton in stock, and if the present high price
continues to be maintained they will be com-
pelled to purchase new stocks at the prevailing
higher rates. The disparity between the price
paid for raw cotton by the larger companies
and that paid by the smaller concerns will pre-
vent the product of the latter from being
placed on the market on an equal footing with
those of the former. In the present state of
business, it is difficult to believe that all the
larger companies are holding large stocks of
raw cotton purchased when quotations were
lower, and even if they are the stocks will be
scarcely enough to last for the remainder of the
year, and the spinners will have to buy new
stocks for next year at the higher prices. The
Osaka Journal then asks whether the smaller
spinning companies will be able to hold their
own against their stronger rivals holding a large
supply of cheap raw cotton, and further asks
whether the yarn market is likely to rise by
the time the large companies have used up
all their stocks of cheap cotton, and they
are compelled to buy supplies at higher
prices. The opinion prevails in some
quarters that the smaller companies will
propose a reduction of working hours in the
spinning-mills and so limit the output of yarn,
in order to force up the market. The larger
companies, which at present see no prospect of
an improvement in the yarn market, might
also agree to the proposal. Japanese spinners
are now in a better position than their Indian
confères, and if the present price of yarn is
maintained, the export may increase, followed
by an advance in the home quotation for yarn.
It remains to be seen, however, whether the
advance in the quotation for yarn can keep
pace with the rise in the price of raw cotton.
It is believed that the Japan Cotton Spinners'
Association will agree to a further suspension
in the operation of spindles, although 25 per
cent are already stopped.

The total export of yarn last month amount-
ed to 1,816 bales, a decrease of 815 bales on
the figures for the preceding month and of 8,370
bales as compared with August last year. The
quotations on the *Osaka Cotton Yarn Ex-
change* on Saturday closed as follows:—

September delivery.....¥28.55
October.....¥28.20
November.....¥28.05

These quotations have been maintained for
some time past with but slight fluctuations;
JAPAN-CHINA SPINNING COMPANY.

The board of directors of the Japan-China
Spinning Company (Nishin Bosai Kaisha)
has adopted a resolution to erect the second
mill of 20,000 spindles for coarse yarn, and to
install 800 weaving-looms. It is proposed to

call up a further ¥200 on each share—amount-
ing to ¥500,000 in all—early in December next
towards the cost of the new mill and plant. The
purchase money for the machinery will be
borrowed from Messrs. Samuel & Co. of
Yokohama, at 6 per cent. It is further propos-
ed to call up another ¥250 on each share in
March next to settle the money now owing by
the company to the foreign firm mentioned,
which falls due next March. The construction
of the second mill is to be commenced in
December next.

Intimations.

NOTICE.

THE VALUE OF THE HONGKONG
DOLLAR as proclaimed by the Director
of the United States Mint for the three months
to end December 31, 1909, is 404 in terms of
American Gold Currency.

Consular Fees for the quarter ending De-
cember 31, 1909, will be as follows:—

Invoice.....\$ 6.25
Extra Copy of Invoice.....2.50
Landing Certificate.....6.25
Bill of Health.....12.40
Supplemental Bill of Health.....6.25

Hongkong Currency only is accepted in
payment of fees at this Consulate General.
The Invoice Clerk is forbidden to accept any
Chinese coin whatever or to accept Hongkong
fractional coin in amounts of over \$3.00 at one
time.

STUART J. FULLER,
American Vice-Consul General
in charge.
Hongkong, 4th October, 1909. [698]

REGRET

You will NEVER if you
VISIT

MOHIDEEN &
THAHA,

in
D'AGUILAR STREET,

the
NEW JEWELLERS
AND DEALERS

in
CEYLON PRECIOUS
STONES

of every description, and
other GEMS.

Hongkong, 31st August, 1909. [610]

of every description, and
other GEMS.

Hongkong, 31st August, 1909. [610]

of every description, and
other GEMS.

Hongkong, 31st August, 1909. [610]

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Intimations.

THE
SOUTH CHINA ARTISTIC
BAZAAR.

EXHIBITION HALL

ON
FIRST FLOOR OF NO. 35 DES VOUTS ROAD
CENTRAL
(opposite the P. & O. S. N. Co.'s office.)

OPENED DAILY:

From 11 A.M. to 5 P.M.
From 7 P.M. to 11 P.M.

Ivory, Silver, China, Paper, Wooden and
Bamboo Ware.
Embroideries, Silk, Oil and Water Colour
Paintings.
Carols and Perfumes.
Screens, Pictures and Photo Frames.
Preserves and Canned Goods.
Writing Inks and Paper, &c.
Hongkong, 25th September, 1909. [675]

JUST LANDED:

The well-known and famous brandy
"Bisquit Dubouche

& Co."

XXX Very Old Fine\$2.50
V.O.C.B. Guaranteed 20 Years
Old 5.50

ALSO

QUINQUINA?

QUINQUINA?

DUBONNET?

FRENCH STORE,

Sole Agent.

Hongkong, 30th April, 1909. [640]

PEAK TRAMWAYS COMPANY
LIMITED.

TIME TABLE.

WEEK DAYS.

7.00 a.m. to 10.00 a.m. Every 10 minutes.
10.00 a.m. to 11.00 a.m. Every 15 minutes.
11.30 a.m. to 12.45 p.m. Every 15 minutes.
12.45 p.m. to 1.15 p.m. Every 10 minutes.
1.15 p.m. to 1.45 p.m. Every 15 minutes.
1.45 p.m. to 2.15 p.m. Every 15 minutes.
2.15 p.m. to 2.45 p.m. Every 15 minutes.
2.45 p.m. to 3.00 p.m. Every 15 minutes.
3.00 p.m. to 3.30 p.m. Every 15 minutes.
3.30 p.m. to 3.50 p.m. Every 15 minutes.

NIGHT GARS.

8.45 p.m. and 9 p.m., 9.45 p.m. to 11.15 p.m.
Every half hour.

SUNDAYS.

8.00 a.m. to 9.00 a.m. Every 15 minutes.
9.00 a.m. to 9.30 a.m. Every 15 minutes.
9.30 a.m. to 10.30 a.m. Every 15 minutes.
10.30 a.m. to 11.00 a.m. Every 15 minutes.
11.45 a.m. to 12.00 noon Every 15 minutes.
12.00 noon to 1.00 p.m. Every 15 minutes.
1.00 p.m. to 1.30 p.m. Every 15 minutes.
1.30 p.m. to 2.00 p.m. Every 15 minutes.
2.00 p.m. to 2.30 p.m. Every 15 minutes.
2.30 p.m. to 3.00 p.m. Every 15 minutes.

NIGHT GARS as on Week Days.

SATURDAYS.

Extra cars at 3.15 p.m., 11.30 p.m. and
11.45 p.m.

SPECIAL

Intimation.



A. S. WATSON & CO., LIMITED

ESTABLISHED A.D. 1841.

AERATED WATER MANUFACTURERS.

SPECIALITIES:

DRY GINGER ALE.

LIME FRUIT CHAMPAGNE.

ORANGE CHAMPAGNE.

STONE GINGER BEER.

PALATABLE

AND

REFRESHING.

Watson's

FRUIT SYRUPS.

mixed with aerated or plain water make excellent refreshing beverages.

Guaranteed to be made from the pure juice of sound ripe fruit.

A. S. WATSON & CO., LIMITED.

HONGKONG AND KOWLOON.

Hongkong, 15th July, 1911.

SUBSCRIPTION RATES ON ADVANCE.

DAILY—286 per annum.

WEEKLY—\$15 per annum.

The rates per quarter and per annum are proportional.

Subscriptions for any period less than one month

will be charged at a full month.

The daily issue is delivered free when the address is

accessible to messenger. Peak subscribers can have

their copies delivered at their residences without

any extra charge. On copies sent by post an

additional \$1.80 per quarter is charged for postage.

The postage on the weekly issue is any part of the

world to 30 cents per quarter.

Single Copies, Daily, ten cents. Weekly, twenty

five cents (for cash only).

DEATH.

Holton.—On the 21st May, 1909, at Samar-

Papua, Herbert Nelson Holton, aged 47,

appendicitis.

The Hongkong Telegraph.

HONGKONG, WEDNESDAY, OCTOBER 6, 1909.

HONGKONG AND SINGAPORE AS TRADE CENTRES.

By a rather interesting method of reasoning one of our Singapore contemporaries regards the opening of the new port of Fishguard to the Atlantic passenger trade as bearing some relation to what the future holds in store for Singapore. The writer is not very insipid in his views and possibly the fact that the article appears in a Saturday's issue, after perhaps a long and weary week, explains the real reason of his evident dependency at the fact that the port is no longer a distributing centre but a mere place of call. As the writer says: "Within the last twenty years we have seen the character of our port change, from being the centre of the coasting trade to a mere calling place for through steamers. The wares that used to come here for distribution to the surrounding countries, and the produce from those countries, are going more and more in bulk direct to and from these developing ports, leaving us with far less local trade but with far more through shipping trade. The process is a natural one

and there seems no reason for supposing that it will cease. Some local trade we shall always have, but it has ceased to be, as regards shipping, the chief feature. On the other hand we lie well situated for all the trade routes of the East. Manufacturing Japan and the United States to the north-east, consuming and manufacturing India on the north-west, and to the south west who can foresee what the future of the South African Union will bring forth? Australia to the south-east, with potentialities, Java with its teeming millions at our door; China with its hundreds of millions of consumers now, and in the future also manufacturers." The picture drawn by our Southern contemporary is not too fanciful, but what precise advantage it is for a port to lose its local trade and gain a passing call from visiting steamers we fail to understand. After all, if the local trade is failing that would seem to indicate that the overseas trade is decreasing and that the important distributing trade is vanishing. These are not pleasant things to suggest but the author of the article seems to find some comfort in them. To quote again: "Our local trade and production and export pale into insignificance by the side of the across-sea trade that will have to pass our doors. It is our business to see that the door is open and inviting and with us prepared to serve at once, and willingly, all comers. If the Gate of the East is to maintain its position it must be always open, its guardians always on the alert. Already conditions have largely changed, and the leisurely dallying with business that befitted a terminus is no longer possible. As a junction, through which so much trade will go, some of it without stopping even, life will be more strenuous. The burden of traffic will be vastly heavier, and will need more attention to see that none of it is lost. This prospect, it seems to us, is the justification for the money we are spending on our harbour works, wharves and docks, and not an insignificant increase to the local trade." Of course the writer may be correct in the view that it is important to provide facilities for vessels, some of which do not even stop at the port, but we fail to see his point. What we should regard as the great desideratum is to bolster up local trade, in other words the trade between adjoining ports so that the great liners in their own interests will be obliged to stop at the central port of collection and distribution. A round-the-world vessel with a cargo of goods for China, Japan and America cannot leave much profit behind even if it does stop at Singapore to coal, but the vessel which visits a port in order to deposit cargo intended for adjacent countries and embark goods which have accumulated at the natural entrepot is both directly and indirectly bringing money to the Colony and, not only that, but it is also contributing towards its material advancement and importance. It is for that and other reasons that we occasionally regard with misgiving the perceptible movement going on around Hongkong, by which the real character of this port may ultimately be completely altered. Already we are in a fair way to lose a considerable portion of our direct trade with Manila by the operation of the new American tariff which declares that American and Philippine goods must pass between the two places—San Francisco and Manila—in one bottom, and, if we are not mistaken, without an intermediate call at any foreign port. We know already that next year or the year after that whatever interland trade British shipping companies may have enjoyed in the Philippine Islands will be restricted to vessels flying the American flag. And so the tale runs on, that the Philippine Islands are to be made a close preserve for American capital. No doubt America is perfectly right in the attitude she has adopted in this respect. She has spent enormous sums on the Islands without reaping a dollar's worth of reward. Money has been sunk there by the hundreds of thousands and there it will stick, while the Filipino patriot keeps on blustering and howling for salaries sufficient to keep himself, his relatives and his retinue of kettledrums in indolence and sloth. But the fact that it is the object of America to keep her "ain fish guts for her ain sea maws" bodes bad for the shipping trade of Hongkong. Then there is Canton, whose strenuous inhabitants are seeking to make the capital of South China the real distributing port of the Empire. Steamers visit Canton without thinking of giving Hongkong a call, and once the delta is dredged so that deep sea vessels can make their way up without danger or hindrance either to Canton or Whampoa, and the grand trunk railway from Canton to Peking via Hankow is in full working order, and adequate facilities for the landing and storing of goods are provided—what then? Well, we are afraid that Hongkong will have to bear the brunt. But why despair? Let us sing paeans of praise and emulate Singapore which regards Japan, the United States and China in the East, India and Europe in the West, Java, Sumatra and Australasia in the South, and Siam, Malaya and Siberia in the North as her subscribing agents of commerce. There is no knowing when something may turn up, something just as satisfactory as the tender for the Opium Farm.

LOCAL AND GENERAL.

SIAMSE buffaloes are being sent to the Philippines to improve their kind in those islands.

Two men were given six weeks' hard labour each to the Police Court this morning for fighting near Jardine's Bazaar.

SHARKS abound off the coast of Negri Sembilan and Malacca and are reported to be causing a deal of damage to the jaring nets.

A CHINAMAN was given six weeks' hard labour at the Magistracy to-day for importing a compatriot into the Colony for the purpose of emigration.

THREE hawkers were to-day fined \$35 each for resisting an Indian policeman. One of the men was fined an extra \$5 for failing to produce his licence when asked to do so.

AMONG recent arrivals in Shanghai have been M. and Madame E. G. Polier, who returned from France by way of Siberia, and who received a very cordial welcome on both sides of the Yang-king-pang.

A TIGER attempted to join a picnic party at Bandar Maria recently, but the ladies shrieked their disapproval of this intrusion and the gentlemen were perforce obliged to drive him off by pelting bottles at him.

MR. M. Hooten, Second Interpreter at the Magistracy, has been promoted First Interpreter in place of Mr. Mackie, who has been transferred back to the Supreme Court. Mr. Sung Tsui Lun takes the position of Second Interpreter.

In connection with the New Territory murder, when two Indian policemen acting as convoys to a treasure-box were set upon by eight Chinamen and brutally murdered near Au Tau, four men appeared at the Magistracy this morning and were formally remanded.

YESTERDAY morning the Chinese about Bow-nington Canal were in a state of great excitement over a little Chinese boy three years old who fell off a sampan and got drowned. The body was afterward picked up by a Sautery Inspector and sent to the mortuary.

The sluice for the coffer dam of the new dry dock at Keppel Harbour, the construction of which is in the hands of Messrs. Topham, Jones and Railton, is closed and the pumps have already started. The Malay fishermen anticipate a harvest, as the place is said to be full of fish.

JARDINE'S BAZAAR was the scene of a disturbance yesterday, as a result of which, two men were each given six weeks' hard labour in the Police Court this morning for assaulting a policeman. On the application of Mr. P. W. Goldring (of Messrs. Goldring, Barlow and Morrell) a rehearing has been granted.

THE N.Y.K. steamer *Yamaguchi*, which arrived at Kobe on the 22nd ultimo from Europe, brought some fine specimens of Highland cattle, two bulls and eleven cows. The price for these animals was about ¥3,000 each. With the exception of one of the bulls, which is destined for the Wakayama pasture, all the animals are to be kept at the Natsusaka pasture in Hiroshima prefecture.

RECENT news from Washington states that Hon. A. P. Wilder, U.S. Consul-General to Shanghai, saw President Taft at Beverly, Mass., and Secretary Knox at Mauch Chunk, Pa., in August, after which he spent some time visiting in Maine. He was expecting to remain with his family for a short time in California, and to leave for Shanghai to take up his duties at an early date.

HARMSTON'S CIRCUS.

PERFORMANCE MARKED BY TORRENTIAL DOWNPOUR.

During the performance at Harmston's Circus last night, the rain came down in bucketfuls and caused a mild consternation among the audience, especially the Chinese portion of it, whose stentorian "hi-yahs" travelled down the gallery with remarkable velocity and could be heard far above the regular pit-patter of the heavy deluge. The water found its way through the canvas in volumes and someone was heard to remark that it was a providential occurrence for those who badly needed a bath. Everybody grumbled about an uncomfortable feeling of a sticky clamminess but with commendable enterprise, the various "turns" were proceeded with as if nothing had happened. The regrettable incident certainly seemed to revive the spirit of the clown, who brought to bear on their buffoonery an unusual vigour which was decidedly refreshing, while the eccentric Emmanuels delighted a wet audience by producing sublime symphony out of their quaint little instruments. The blisful disregard of the artists to extraneous circumstances compensated for the thorough drenching of all and sundry and we can only wish the management more propitious weather conditions next time.

INTERPORT CRICKET.

OPENING DAY.

We are informed that the Straits and Shanghai teams are expected here about the 18th November and the Interport matches will probably start on Monday, the 22nd.

The later date of arrival (previously fixed for the 11th November) has been arranged to prevent clashing with the Volunteer Camp.

RUBBER COMPANY DIVIDENDS.

The following interim dividends are announced—
Golden Hope Rubber Estate, Ltd., 8 per cent.
Bekit Rajah Rubber Co. Ltd., 15
The Federated Selangor Rubber Co., Ltd., has declared a dividend of 25 per cent and the Klang Produce Co., Ltd., an interim dividend of 10 per cent.

CORRESPONDENCE.

(We do not necessarily endorse the opinions expressed by Correspondents in this column.)

EXCHANGE BROKERS' ASSOCIATION.

To the Editor of the "Hongkong Telegraph."

Sir,—It has been an open secret that, during the past few weeks, a movement has been revived by the Exchange Brokers of Hongkong to form an association of their fraternity for the purpose of framing regulations for the guidance and control of exchange brokers in their relations with the Banks. The movement is not a novel one. Tentative attempts had been made in the past to establish such an association, but through inherent difficulties the project failed to fructify.

Another attempt is again being made and it is to be hoped that, like similar previous endeavours, it will be doomed to failure.

The ostensible objects of the Association are—

1st.—To put a stop to the system of rebates granted by brokers; and

2ndly.—To restrict the admission of new brokers by limiting their number and enforcing prohibitive regulations against the advent of newcomers.

As to the first of these objects its impracticability will defeat its own end. Reference need only be made to the system of rebates obtaining with shipping companies to convince the promoters of the Association that it would be well-nigh impossible to bring home to any defaulter the charge of granting secret rebates.

With regard to the second object, it stands self-condemned because of the dog-in-the-manger spirit which it breathes. Competition, it has been asserted, is the soul of trade, and the policy should be one of "Live and let live." Surely, the Exchange Brokers now serving their large number of constituents are sufficiently shrewd men of business to be able to look after themselves with a fair field and no favour, instead of advocating the "closed door" policy against which in relation to commerce in general the exchange brokers have been found to be the most clamorous denouncers.

Had there been a plethora of bullion brokers in Hongkong, the force of the argument, though not admitted, might be allowed a fair hearing, but it so happens that numerically the strength of the exchange brokers' fraternity in Hongkong is at present below that obtaining five years ago.

It will be argued that if the brokers chose to band themselves into a protective association that is their own business and should concern nobody else. In rebuttal of that statement, it is advanced that the mercantile firms and commission agents are likely to be better served when no clique exists and the brokers operate in active and legitimate competition against one another to serve the interests of their principals.

As the recognised exponent of commercial interests in Hongkong, you will, perhaps, permit me, Mr. Editor, to offer a few practical suggestions to those who are helping along this Association scheme. The proposals I wish to put forward are—

1.—That, before proceeding with the formation of the Association, a meeting should be called of all those interested in the subject and the question thrashed out in all its bearings.

2.—That equality of rights and privileges be accorded to all alike.

3.—Is it intended that Banks which transact business with non-members of the Association and allow them brokerage be mulcted in any penalty? If so, in what manner will the committee of the proposed Association enforce the penalty?

4.—Is it permissible, under the rules of the Association, for Banks to transact business direct with assistants of firms instead of through the medium of brokers? How can the rules of the Association be made to govern Bank managers and accountants?

5.—Will Banks be debarred from buying bills direct from assistants of firms?

6.—That it is desirable that all representative firms without distinction as to nationality be eligible on the committee.

The subject is one fraught with innumerable difficulties and from the point of view of one not immediately affected by the proposed organisation, it would appear that the embryonic Association will meet with considerable opposition from a community like that of Hongkong.—Yours, etc.,

MERCHANT.

Hongkong, 6th October, 1909.

A STORM IN A TEA-POT.

INDIAN CONSTABLE ARRESTS SCHOOL-BOY.

It was only yesterday that an Indian policeman was fined \$75 for bringing a false charge against a Chinaman, and at the Magistracy this morning, another illustration of the overzealousness of the turbaned guardians of the peace was provided, when an elderly Chinese school-boy was summoned before Mr. F. A. Hazeland (First Police Magistrate) for disorderly conduct. Dr. Ho Kai, M.B., C.M.C., appeared for the defendant.

It appears that shortly after five o'clock yesterday afternoon, a number of school-boys were playing cricket in a side-street at Tai-ping-shan, when an Indian police constable (complainant) appeared on the scene and is alleged to have deliberately knocked down the stumps, much to the annoyance of one of the boys, who demanded an explanation, and, according to the constable's story, is alleged to have treated the policeman to a few unflattering remarks. The complainant on this is said to have left the scene of the trouble and shortly afterwards appeared with a stick. The boy who had routed the constable's ire, who appears to be a lad of tender years, sought refuge behind the defendant. What happened afterwards is not known, but the constable is alleged to have seized the older of the two boys by the queue and taken him to headquarters.

His Worship discharged the defendant.

JOURNEY ACROSS SIBERIA.

A HONGKONG RESIDENT'S IMPRESSIONS.

It is out of the question within the limited space available to do more than briefly record a few general impressions of what one sees during the day and it is equally impossible to attempt an estimate of the enormous acreage of land under tillage on either side of the Great Siberian Trans-Siberian Railway. Passing over this journey from Hankow, Peking, Mukden and Harbin to Moscow early in May, one only sees signs of agricultural life common to spring time at home. Now in early September both east and west of the Ural Mountains we see those recently harvested lands again ploughed and sown with what will be their spring crop of wheat and which I am told brings forth superior grain to that yielded in the summer crops.

On our homeward bound journey from China in the latter part of April, 1909, the Russian emigrants—men, women and children—were seen in great numbers coming from European Russia in trains with an average of 45 cars—one train had as many as 60 cars—filled with old and young of both sexes; all looking fairly strong and hearty. Though the lot of these poor peasants is a very hard one, to their credit it should be stated that they seem fully prepared to face the hard toil that lies before them. They were all bound eastwards to plough, sow and reap the produce of their labours that will help to supply the staple of life to some of Russia's millions.

Considering the long period of cold weather and other conditions under which these simple peasant folk live, one need not be surprised to find the writer of opinion that those country people do not convey the impression that they are as cheerful and contented with their lot in life as their Chinese neighbours, who, I firmly believe, do possess not only more cheerfulness but a greater spirit of independence.

On our return in September, and going eastward along this great European-Siberian track, as far as the eye could see, one beheld the enormous amount of agricultural work that had been accomplished during the five months that have elapsed from the time of our journey homeward. Evidently these hardy peasants, like the Chinese, do not entertain the fallacy that, irrespective of merit, the world owes them a living; therefore, during the five months he buckles on the armour of labour whilst the weather is genial.

In speeding through Siberia one finds a change of scenery from ploughed fields to the various colours of the autumn leaves of the forest trees, such as pine, willow, birch, larch, poplar, etc. These afford an interesting sight to all lovers of nature. Even the grasses are variegated in colour, and the setting sun adds so much to make the picture complete.

The churches are a great feature of rural peasant life. Without these among the villages, the inhabitants of this vast territory—especially the women—would feel miserable so far removed from civilization. These churches are built of wood—I refer to those in the Siberian villages—are octagonal in shape, and painted in various shades of green. The domes and crosses are, with a rising or setting sun, an impressive sight, the crucifixes, and in many cases the domes, are richly gilded with gold leaf.

Fifteen days after leaving London or Dover found us at Peking. In the journey from Harbin through Kwang-chun-tze, Mukden and Peking one cannot fail to observe the striking contrast that presents itself, even to those observers who only get a mere glimpse in passing, with regard to the plodding industry of the Manchurian Chinese and the Russian peasants. The former, one must admit, are favoured with longer periods of a brighter and a more genial climate, and we all know what a great influence climate has on races, so that if our Russian friends, the country people, could exchange places with their Chinese neighbours their lives and conditions might conduce to harder work and more cheerfulness. Coming south from Harbin through Manchuria to Peking every bit of land that was favourable to cultivation was a revelation as well as a confirmation of the old saying that for agricultural work the Chinese are about the best in the world.

Pere in Manchuria they raise many of the best kinds of cereals, such as kowling (millet), showme or small millet, wheat, buck wheat, beans, peas, etc. For two days or more traversing this country we were continually passing fields of cotton, the gathering of which was being done by women and children, the latter very young and during the warmth of day were clothed in nature's garment without the fig leaf.

From Peking to Hankow by railway a distance of 700 miles (approximately) the same or similar conditions of agricultural life were ever present; and the people down to the little youngsters of 4 and 5 are seen plodding along till after sundown. From Hankow to Shanghai we came down the Yangtze on one of the palatial steamers on board which the stewards fed you five times a day; and at breakfast,iffin and dinner are attended with as much care as a fond mother watches over her sick child. What food mother watches over her sick child. What food mother watches over her sick child. What food mother watches over her sick child.

I have mentioned that in crossing Siberia homewards in the early spring of this year we saw great numbers of emigrants going towards Siberia. By the length and number of trains we variously estimated them to carry about 12,000 every day. These emigrants, we were informed, were assisted with a free passage by the State, and the State also provides at every station day and night a plentiful and good supply of boiling water free.

Black, white and brown bread, milk and butter and other foodstuffs are fairly cheap and plentiful. We were further informed that the Russian authorities are doing a good deal to induce emigration to Eastern Siberia by giving free grants of land, etc. If this be the case, it should, in a few years, prove a great benefit to the Russian country.

From Shanghai to Hongkong in a most comfortable and well appointed steamer, a trust-worthy and genial skipper as well as officers and engineers a pleasant trip from Dover via Siberia to Hongkong, was brought to an end. That portion of the journey through Manchuria, China (Peking) to Hankow, Hupoh, and to Shanghai by the great Yangtze was the most interesting owing to the many changes of scenery and the busy life of its people.

T. R. H.

Telegram.

"HONGKONG TELEGRAPH" SERVICE.

LORD KITCHENER.

DEPARTURE FROM SHANGHAI.

[From Our Own Correspondent.]

Shanghai, 6th October,

4.30 a.m.

Lord Kitchener, accompanied by Lieut.-Colonel C. D. Bruce, left by special train for Nanking this afternoon.

There were present at the railway station the members of the Consular Body, Mr. D. Landale, Chairman of the Municipal Council, and other notable personages.

COMPANY LAW IN HONGKONG.

PROSECUTION BY THE REGIS AR.

At the Magistracy, this afternoon, before Mr. J. R. Wood (Second Police Magistrate) the Kwong Kee Ferry Company, Limited, of 152, Queen's Road Central, and the Kwong Shan Steamboat Company, Limited, of 178, Queen's Road Central, were prosecuted for not complying with certain provisions required by the Companies Act of 1885, i.e., not filing the returns of shareholders.

Mr. H. L. Deauys, Esq., (from the Crown Solicitor's Office) prosecuted; and Mr. F. Paget-Hell (of Messrs. Britton and Hell) represented the defendants.

Mr. J. R. Kemp, Registrar of Companies, stated that the Kwong Shan Company was registered in the Company Register. The Memorandum of Association was registered on the 20th May, 1904. In that year, no summary of shareholders was forwarded. The summary for 1905 was forwarded, and also for 1906 and 1907. No summary was filed for 1908. The summary purported to be made up to 27th August, 1908, was filed on 17th September, 1909, whereas it ought to have been registered on 31st August, 1908. The registered capital of the company was \$500,000. Witness could not say whether there had been similar prosecutions before. He did not know whether or not notice was sent.

In the Kwong Kee Company's case, it was stated that the company was registered in the local Register. The amount of the capital was \$25,000. The Memorandum of Association was registered on the 16th September, 1904. The first time the list of shareholders was filed was on the 24th August, 1906. The return for 1906 was filed on the same day, that for 1907 on the 14th November, 1908, and for 1908 on the 27th September, 1909. The last return should have been filed on the 11th August, 1909.

Mr. Deauys stated that the maximum penalty was \$100 but asked His Worship to inflict a fine of two per cent. on the full penalty.

The cases were adjourned sine die.

AN EXEMPLARY SENTENCE.

ONE YEAR'S IMPRISONMENT FOR A SHEET-SNATCHER.

The snatching of gold ear-picks from Chinese women by unscrupulous thieves has of late become a glaring nuisance and the fact that a man sharp sentences have been meted out at the Magistracy to the delinquents has not mitigated the nefarious practice to any appreciable extent. Before Mr. J. R. Wood (Second Police Magistrate) in the Police Court this morning, one of the fraternity appeared on a charge of snatching a gold ear-pick from a Chinese woman's head. According to the story of the prosecution, the complainant, the wife of a schoolmaster, was walking along Queen's Road West, when she suddenly felt her gold ear-pick being snatched from her head, and on turning round, she saw the defendant running away with the article. In his hurry to get away, the man dropped the pick, which the woman at once picked up. The school-master, who accompanied his wife, then gave chase to the defendant, whom he soon captured and gave into the custody of the police. At the Magistracy this morning, the defendant was sentenced to one year's imprisonment and six hours' stocks in lieu of one day.

TIGER AT LARGE.

BOLTS FROM A MENAGERIE AND CAUSED PANIC IN A TOWN.

Lisbon, Sept. 3. The paper *Liberal*, reports that at the annual fair at Clervaux, a menagerie of various ferocious animals, including a full-grown tiger, was on exhibition.

After the performance yesterday, the tame lion, the door of the tiger's cage insecurely fastened, with the result that while the audience were trooping out the tiger pushed the door open and, with a tremendous roar, rushed out. The panic among the audience may be easily imagined—men shouting and women and children shrieking.

The tiger, however, probably frightened by the uproar, took several great bounds, leaped over the audience, and bolted through the door into the street. Great confusion prevailed, the towns, the terror-struck population flying in all directions. A force of Civil Guards was formed up to shoot the tiger at sight. The owner, however, not wishing for the death of his valuable animal, offered to head the chase. The guards gladly consented, and with the help of several of its assistants, the owner finally succeeded in lassoing the animal, though several persons were mangled by the tiger.

Telegrams.

"HONGKONG TELEGRAPH" SERVICE.

BUTLER WRIGHT CASE.

TRIAL TAKES PLACE IN CANTON.

APPLICATION FOR CHANGE OF VENUE TO SHANGHAI REFUSED.

[From Our Own Correspondent.]

Shanghai, 6th October, 12.50 p.m.

Sir Havilland de Saumarez, Chief Justice, delivered his decision on the application made by Mr. J. C. E. Douglas, representing Mr. Butler Wright, chief accountant of the Canton-Kowloon railway (Chinese section), that the venue of trial in the action brought against the accused charging him with embezzlement of railway funds, be changed from Canton to Shanghai.

The ground of appeal was that Mr. Butler Wright feared he would be unable to get an impartial jury in Canton.

The Chief Justice has now decided that the trial must take place at Canton and the application for a change of venue to Shanghai was accordingly refused.

In the course of his remarks in delivering the decision his Lordship commented on the reports which had appeared in certain Hongkong newspapers with reference to the circumstances surrounding the departure of Mr. Butler Wright from Canton in the first instance.

KITCHENER IN SHANGHAI.

INSPECTION OF VOLUNTEER CORPS.

EULOGISTIC APPRECIATION OF THE VARIOUS UNITS.

[From Our Own Correspondent.]

Shanghai, 6th October, 12.50 p.m.

Last evening Lord Kitchener inspected the various companies of the Shanghai Volunteer Corps.

Afterwards, the Field Marshal expressed his appreciation of the appearance of the men in the most eulogistic terms.

BOYCOTT PROPAGANDA.

INSTRUCTION TO TELEGRAPH OFFICES.

[By courtesy of the "Shing Po"]

Peking, 5th October.

The Waiyup has instructed all the telegraph offices to exercise more care in the transmission of telegrams having reference to any anti-foreign boycott propaganda.

OBITUARY.

PASSING OF CHANG CHIH-TUNG.

[By courtesy of the "Shing Po"]

Peking, 5th October.

On account of the railway loan agreements which he desired to have cancelled and in which the other Grand Councillors did not concur, Chang Chih-tung's illness has become aggravated.

Later.

Chang Chih-tung's illness has taken a serious turn and he is making preparations for the approaching end.

Later.

Chang Chih-tung is dead. He expired at midnight on the 4th inst.

TYPHOON WARNING.

The telegram quoted below was received at the American Consulate-General from the Manila Observatory at 10.15 a.m. to-day. October 6th, 9 a.m. Cyclone or typhoon, W. of Bellona Channel moving W.

CANTON DAY BY DAY.

ANTI JAPANESE BOYCOTT.

[From Our Own Correspondent.]

Canton, 5th October.

In accordance with instructions received from the Viceroy, H.E. Yuan Shu Hsueh, the Taoist of Constabulary, Wong Ping Pi, addressed a communication to the Canton Self-Government Society to inquire whether the Society has convened any public meetings or issued any circulars to stir up the feeling of the people against the Japanese over the recent convention relating to the Three Eastern Provinces, with a view to revive the Japanese boycott movement. In answer to the despatch the Self-Government Society states that it has not taken any such action as alleged in seeking to revive the Japanese boycott in this city.

EX-TREASURER'S DEPARTURE.

The ex-Acting Viceroy in Canton, H.E. Wu Seung Lum, who was permitted to resign from the Kwangtung Provincial Treasury, left here this afternoon on board the gunboat *Po Pi* for Hongkong where the retired official will tranship for Shanghai by the C. M. S. N. Co.'s steamer *Kwangtshen* en route for his native province of Kiangsi. He left behind him here his fifth son who is now studying in the Canton Provincial College of modern languages. Many officials of this city were present at the Government Tien Tze wharf to see the retired official off.

GERMAN CONSUL.

Dr. Rossler, German Consul at Canton, who was absent on leave for several weeks, has returned and resumed charge of the office at the German Consulate a few days ago.

DEATH OF CAPT. YUENG.

Yeung Hung Piu alias Yeung Chik Sang, formerly Commander of the Shamen guards, who had a large circle of foreign acquaintances in this city as well as in Hongkong and who was removed from office only a few months ago by the ex-Viceroy, H.E. Chang Jen Chuan, died on the 2nd inst. in Canton. Yeung Hung Piu was taken ill only a week or so before he died.

MACAO'S DELIMITATION.

H.E. Viceroy Yuan has instructed Expectant Magistrate Chan Pak Hau to assist the Delimitation Commissioner, H.E. Ko Yu Him, in conducting the negotiations with the Portuguese Delimitation Commissioner on the Macao boundary question.

ROYAL HONGKONG GOLF CLUB.

The Captain's Cup and Pool were played for, from 2nd to 4th October. The following scores were returned:—

CAPTAIN'S CUP.		
	Gross Handicap.	Net.
W. M. Anderson	92	7 85
J. Johnston	91	4 87
R. P. Walker	104	14 90
19 entries. Remainder no return.		
POOL.		
	Gross Handicap.	Net.
M. A. Murray	87	5 82
J. R. Boyes	101	18 83
W. M. Anderson	92	7 85
W. Innes	93	8 85
R. P. Walker	104	14 90
19 entries. Remainder no return.		
Winner of Cup.		
Winner of Pool.		

PARIS TO SAIGON.

PARING TRIP ON AN AIRPLANE CONTEMPLATED.

M. Deloncle, who is member for Cochinchina in the French Parliament, and who passed through Singapore recently, is now at Saigon on a visit to his constituents. He is an enthusiast in flying, and attended a meeting of the Airship Society in that city on September 10, and spoke in favour of at once experimenting with aeroplanes on the Bliet system. It is estimated that the machines would cost about 12,000 francs each and towards this amount a considerable sum has been raised. M. Deloncle's view is to construct an aeroplane at Paris and, in the near future, organise a race from the French capital to Saigon which, he declares, should take about twelve days. The prize could be set at 250,000 francs. One of the speakers said that the Colony could easily raise 100,000 francs towards this amount. M. Deloncle's scheme was received favourably and a Committee was appointed to settle the details.

THE LONG DISTANCE PROBLEM.

M. Deloncle, in unfolding his plan, said that the likelihood of such a long flight was recently discussed in Paris. He dwelt upon the fact that Bangkok was moving in the matter of flying, and had opened negotiations with M. Bliet. The idea at present was that the Paris-Saigon flight should be accomplished in the daytime only, there being stoppages at night. The time table could be arranged as follows:—Paris, Zurich, Vienna, Buda-Pesth, Sofia, Constantinople, Koniak, Bagdad, Bushire, Kurrachee, Benares, Calcutta, Rangoon, Bangkok and Saigon. Should shipping co-operation be secured, a less direct route could be marked out by way of Marseilles, Corsica, Messina, Crete, Jerusalem, Damascus, Bagdad, and the places following this city in the former itinerary. M. Deloncle, in short, seeks to solve the problem of flying to great distances.

SPEED TO BE REACHED.

He mentioned conversations with leading aeronauts in France who thought nothing of speeds which may shortly reach 400 kilometres (120 miles) an hour. M. Lissard goes further and forecasts speeds ranging from 1,000 to 1,500 kilometres an hour, but M. Deloncle is content with a speed of 100 kilometres an hour for the proposed race. He holds out hopes of contributions from foreign governments and from the newspaper press.

For the present, the Saigon branch will begin by laying out an airship course to conduct experiments with a Bliet aeroplane. The local press will be kept fully informed of the progress made.—*Shing Po*.

FOOTBALL.

R. O. C. V. NAVAL YARD.

Yesterday afternoon the above teams met in a friendly game of football at the Naval Ground. This was the first match the Boys played this season and it resulted well for them. The forwards played a good combination game, and we suggest that they should cut out the selfish play as it will never do any team any good. It could be seen yesterday that one or two of the players kept the leather to themselves.

In the first half the Boys kept harassing their opponents' goal and Blackmore opposed the score with a hard-swing which beat the goalkeeper. Not long after this Y. Abbas netted the second goal and was followed again by Blackmore with a third. The Yards tried hard to score, but nothing resulted.

In the second half things looked bad as the Navy men scored two goals one after another; this was done by Coxon and Crowley. The Boys then played a steady game attacking the Yards' goal time after time and the ball was well played by Chunyit to Parlow, who scored the last and fourth goal. The game ended in a victory for the B.O.C. by 4 goals to 2.

THE KIUKIANG BOYCOTT.

REPRESSIVE PROCLAMATIONS.

The following is a translation of the latest proclamation issued in connection with the boycott. We understand that copies of it are to be posted throughout the whole Yangtze valley. Since the merchants are alleged to be blameless, it only remains to ensure that the "lawless persons" shall be discovered, and that the promised punishment be indeed inflicted. We may add that the phrase "the fatal assault by the British policeman Mears" is the Governor's reading of the incident, not ours.

BY FENG & CO. & CO., GOVERNOR OF KIANGSI, A MOST STRINGENT AND SPECIAL PROCLAMATION.

Having heard that the merchants of Kiukiang on account of the fatal assault by the British policeman Mears on Yu Fa-cheng had refused to ship merchandise by British steamers, a most improper proceeding, I sent repeated telegraphic instructions to the Kiukiang Tao-tai to make strict investigation with the Prefect and Magistrate and proclamations were twice issued prohibiting (the boycott). I also despatched an able deputy to proceed at once to Kiukiang to remonstrate urgently with the Chamber of Commerce.

I myself have now come to Kiukiang on official business, and have personally made secret inquiries and find that the merchants have not as yet actually decided to boycott British shipping, but it appears that Kiukiang being a busy mart situated right on the bank of the Yangtze there are inevitably a number of bad characters about who have taken this opportunity to create trouble and spread rumours, so that merchants are disturbed in mind and afraid to come on to the concession to take delivery of cargo, a proceeding which shows disregard of our Government's international relations and of its care for the merchant class. It becomes my duty therefore at once to issue a stringent prohibitory proclamation.

All you merchants and people of the port of Kiukiang must know that the case of the fatal assault by the British policeman Mears on Yu Fa-cheng has already been several times reported by me by telegraph to the Waiyup and commended to their earnest attention and it must of course be dealt with justly and according to the Treaties.

You who are all acquainted with the great principles must also know that to conspire together to institute a boycott is contrary to law; moreover, the friendship between China and Great Britain being of long standing and many years of commercial intercourse having inspired mutual confidence, there can be no ground whatever for distrust.

From this date onward you must carry on your business as usual and not listen to rumours which will bring you into conflict with the laws. Should there be any lawless persons who spread rumours and by secret incitement seek to cause trouble, they will on discovery be arrested and severely punished and it is open to any one to denounce such persons. No leniency whatever will be shown and any disobedience to this proclamation will be visited with instant punishment.

Kiukiang, September 22, 1909.

A FURTHER PROCLAMATION.

The following is a full translation of the second proclamation issued by his Honour Tsai Nai-hung, Shanghai Tao-tai, against any extension of the Kiukiang boycott to Shanghai. It will be noticed that the language used is commendably stricter:—

With reference to the boycotting of British shipping at Kiukiang in Kiangsi, I received lately a dispatch from the British Consul-General and thereupon issued a Proclamation notifying our Chinese mercantile class that they were not to discriminate unfairly against British ships. Contrary to my expectation this boycotting has reacted on Shanghai and I am now informed that no goods from Shanghai are now being shipped by British vessels. This is a great injury to commerce. I am accordingly issuing a Proclamation in more stringent terms.

Therefore notice is hereby given to all you Chinese merchants in Shanghai, and you are hereby notified that the Governor of Kiangsi has issued a Proclamation forbidding this boycotting and that I also am in receipt of telegraphic instructions from the Waiyup to issue a similar Proclamation strictly forbidding it. The trade between Great Britain and China is greater at Shanghai than elsewhere, and merchandise should be carried as hitherto in British vessels, and no disobedience to the order of our Government should occur. You also should not listen to rumours from Kiukiang and be led into such foolish action, which will involve you, too, in legal penalties.

Let no one say now that he has not been warned, but let all take heed not to disobey this Proclamation.

—H. C. D. News.

September 25, 1909.

Today's Advertisements.

HONGKONG JOCKEY CLUB.

NOTICE.

AN EXTRAORDINARY GENERAL MEETING OF MEMBERS of the above Club will be held on SATURDAY, the 23rd October, 1909, at 12.15 P.M., at the Offices of the Jockey Club on the Ground Floor of the Hongkong Club Annex, Chater Road, a notice regarding which is being sent to each Member.

By Order,
T. F. HOUGH,
Clerk of the Course.
Hongkong, 6th October, 1909. [795]

NORDDEUTSCHER LLOYD, BREMEN, IMPERIAL GERMAN MAIL LINE.
FOR SHANGHAI, NAGASAKI, KOBE AND YOKOHAMA.

THE Imperial German Mail Steamship

"PRINZ-REGENT LUITPOLD,"
Captain H. Kirchner, will leave for the above places TO-MORROW, the 7th inst., at 6 A.M. For further particulars, apply to

NORDDEUTSCHER LLOYD, MELCHERS & Co., General Agents.
Hongkong, 6th October, 1909. [5]

NORDDEUTSCHER LLOYD, BREMEN, IMPERIAL GERMAN MAIL LINE.

NOTICE TO CONSIGNEES.

THE Steamship

"PRINZ-REGENT LUITPOLD,"
having arrived, Consignees of Cargo are hereby informed that their Goods, with the exception of Opium, Treasure and Valuables, are being landed and stored at their risk into the Godowns of the Hongkong and Kowloon Wharf and Godown Company, Limited, at Kowloon, and West Point Godowns, whence delivery may be obtained.

No Claims will be admitted after the Goods have left the Godowns, and all Goods remaining undelivered after the 13th of October, will be subject to rent.

All broken, chafed, and damaged Goods are to be left in the Godowns, where they will be examined on the 13th of October; at 9.30 A.M. All Claims must reach us before the 17th of October, 1909, or they will not be recognised. No Fire Insurance will be effected. Bills of Lading will be countersigned by the Undersigned.

THIS STEAMER BRINGS CARGO.
Ex S.S. *Alberto Treves* from Venice.
NORDDEUTSCHER LLOYD.

MELCHERS & Co., General Agents.
Hongkong, 6th October, 1909. [6]

LEGISLATIVE COUNCIL.

A LONG AGENDA.

A meeting of the Legislative Council will be held on Thursday next, at 2.30 p.m. Following is the agenda list:—

Financial Minutes. (Nos. 41 and 42).
Resolution under Section 6 of the Liquor Licences Extension Ordinances, 1908.
Market Bye-laws under the Public Health and Buildings Ordinances, 1903-1909.

Third reading of the Bill entitled An Ordinance to amend the Malicious Damage Ordinance, 1865.

Third reading of the Bill entitled An Ordinance to relieve the Governor-in-Council of certain duties.

First reading of a Bill entitled An Ordinance to apply a sum not exceeding Five million six hundred and twenty-five thousand six hundred and eighty-three Dollars to the Public Service of the year 1910.

First reading of a Bill entitled An Ordinance to amend the Liquors Ordinance, 1909.

First reading of a Bill entitled An Ordinance to provide for the periodical inspection of Steam Boilers and Prime Movers.

First reading of a Bill entitled An Ordinance to amend the Widows' and Orphans' Pension Ordinance, 1908.

First reading of a Bill entitled An Ordinance to amend the Protection of Women and Girls Ordinance, 1907, as amended by the Protection of Women and Girls Amendment Ordinance, 1905.

First reading of a Bill entitled An Ordinance to amend the Law relating to Trade Marks.

First reading of a Bill entitled An Ordinance to provide for the Reservation of certain lands in Victoria, in the Peak District, and in Kowloon as Recreation Grounds, and to provide for Regulations as to the use thereof.

First reading of a Bill entitled An Ordinance to amend the Public Places Regulation Ordinance, 1870.

First reading of a Bill entitled An Ordinance to amend the Chinese Extradition Ordinance, 1889.

Second reading of the Bill entitled An Ordinance to authorize the Construction and Maintenance of a Harbour of Refuge upon and over certain portions of the Sea Bed Foreshore situated upon the Harbour frontage at Taikot-tai, Mongkok-tai, and Yau-mai, Kowloon, in this Colony.

Second reading of the Bill entitled An Ordinance to amend the Tramway Ordinance, 1902.

Second reading of the Bill entitled An Ordinance to amend the Liquor Licences Extension Ordinance, 1908, and the Liquor Licences Extension Ordinance, 1908, and to repeal the Liquor Licences Amendment Ordinance, 1908.

Committee on the Bill entitled An Ordinance to set apart certain Crown Land to be used as a burial ground for persons professing the Christian Religion, other than members of the Roman Catholic Church.

C. CLEMENTI,
Clerk of Councils.

* Will not be proceeded with at this meeting.
N.B.—A meeting of the Finance Committee will be held immediately after the Council.

Today's Advertisements.

AMERICAN AND MANCHURIAN LINE.

NOTICE TO CONSIGNEES.

FROM NEW YORK AND SINGAPORE.

THE Steamship

"BLOEMFONTEIN,"
Captain Linklater, having arrived from the above Ports, Consignees of Cargo are hereby informed that their Goods are being landed at their risk into the Godowns of the Hongkong and Kowloon Wharf and Godown Company, Limited, Kowloon, and stored at Consignees' risk and expense.

All broken, chafed, and damaged Goods are to be left in the Godowns, where they will be examined on TUESDAY, 12th inst., at 3 P.M. All Claims must be presented within fifteen days of the steamer's arrival here, after which date they cannot be recognized.

No Claims will be admitted after the Goods have left the Godowns, and all Goods remaining undelivered after the 13th inst. will be subject to rent.

No Fire Insurance has been effected. Bills of Lading will be countersigned by

SHEWAN, TOMES & Co., Agents.
Hongkong, 6th October, 1909. [793]

NOTICE TO CONSIGNEES.

FROM CALCUTTA, PENANG AND SINGAPORE.

THE Steamship

"LIGHTNING,"
having arrived from the above Ports, Consignees of Cargo are hereby informed that their Goods will be delivered from alongside.

Cargo impeding the discharge will be landed at once, at Consignees' risk and expense.

Cargo remaining on board after 4 P.M., of the 8th inst., will be landed at Consignees' risk and expense.

Consignees of Cargo from SINGAPORE and PENANG are requested to take IMMEDIATE DELIVERY of their Goods from alongside, such Cargo impeding the discharge of the vessel will be landed and stored at Consignees' risk and expense.

No Fire Insurance has been effected. Bills of Lading will be countersigned by the Undersigned.

DAVID SASSOON & Co., LIMITED, Agents.
Hongkong, 6th October, 1909. [796]

HARMSTON'S GRAND CIRCUS

AND ROYAL MENAGERIE OF PERFORMING WILD ANIMALS.

THE LARGEST AND BEST CIRCUS COMBINATION TRAVELLING THE EAST.

Location of our Tents:—

TRAMWAY TERMINUS, KENNEDY TOWN.

Again To-night) at 9.15 sharp.
Again To-night)

Our all Star Company of
LONDON & CONTINENTAL ARTISTES.

NEXT MATINEE

SATURDAY, October 9th.

DOORS OPEN AT 3 O'CLOCK. PERFORMANCE AT 4 SHARP.

(Children Half-price at Matinee only).

Same Great Programme as in the Evening—Nothing curtailed.

SPECIAL PERFORMANCE: SUNDAY NIGHT.

For Full particulars read our Descriptive Hand Bills.

Military under the rank of an Officer half-price to the \$1.00 and \$2.00 seats.

Booking at ROBINSON PIANO Co., Ltd.

NOTICE—Special Trains running before and after performance.

MADAME HARMSTON-LOVE,

COL. BOB LOVE,

R. ALTON,

Proprietors.

Sole Manager.

Advance Representative.

Hongkong, 6th October, 1909.

1884

CLUB WHISKY

AGE, QUALITY

AND

MELLOWNESS.

Test for 15 years as an Ideal Scotch for this

climate.

PRICES ON APPLICATION.

H. PRICE & CO., LD.,

WINE MERCHANTS,

12, Queen's Road Central.

Telephone No. 133.



COMMERCIAL.

TO-DAY'S EXCHANGE.

Selling.	
London-Bank T.T.	110
Do. demand	109 1/2
Do. 4 months sight	109 3/4
France-Bank T.T.	230
America-Bank T.T.	42
Germany-Bank T.T.	178
India T.T.	11
Do. demand	11 1/2
Shanghai-Bank T.T.	74 1/2
Singapore-Bank T.T. per H.K. 100	43 1/2
Japan-Bank T.T.	55 1/2
Java-Bank T.T.	105 1/2

Buying.

4 months sight L/O	110 1/2
6 months sight L/O	111 1/2
30 days sight San Francisco & New York	43 1/2
4 months sight do.	44 1/2
30 days sight Sydney & Melbourne	109 1/2
4 months sight France	230 1/2
6 months sight do.	231 1/2
4 months sight Germany	178 1/2
Bar Silver	35 1/2
Bank of England rate	21 1/2
Sovereign	114 1/2

SHIPPING AND MAILS.

MAILS DUE.

Canadian (Empress of Japan) 7th inst.	Indian (Kutnam) 7th inst.
French (Tourane) 11th inst.	Indian (Laitang) 16th inst.

The s.s. *Eastern* left Kobe yesterday for this port and is due here on 10th inst.

The C.N. Co's s.s. *Tien* left Shanghai on 6th inst. and is due here on 9th inst.

The C.N. Co's s.s. *Changsha* left Sydney on 5th inst. and is due here on 18th inst.

The s.s. *Hellipolis* left Durban on 30th ult. for Chingwang and this port, and is expected here on 21st inst.

The C.P.R. Co's s.s. *Empress of India* left Yokohama on 5th inst., at noon, for Victoria and Vancouver, B.C.

The M.M. Co's s.s. *Touman*, with French mail of the 12th ult., will leave Saigon on 7th inst., at midnight, and is expected to arrive here on or about the 10th inst., at 6 p.m., and will leave for Shanghai and Japan on the afternoon of the 11th inst.

THE WEATHER.

The following report is from Mr. F. G. Figg, Director of the Hongkong Observatory:

On the 5th at 6 p.m.—The Pacific depression is near the Balingtang Channel, apparently moving westwards.

On the 6th at 11.55 a.m.—The barometer has risen moderately over China, and fallen rapidly in E. Japan.

The recent typhoon has entered Tongking.

The other typhoon has moved westwards through the Balingtang Channel and entered the China Sea.

The northern depression, which has continued to move westwards, is crossing N. Japan.

An area of high pressure covers N. China, and the monsoon has set in strongly in the North.

N.E. gales may be expected in the Formosa Channel, and bad weather over the N.E. part of the China Sea.

Hongkong Rainfall for the 24 hours ending at 10 a.m. to-day, 0.70 inches.

FORECAST.

- 1.—Hongkong and Neighbourhood, N. winds, strong; fair to squally and unsettled.
- 2.—Formosa Channel, N.E. gale.
- 3.—South coast of China between Hongkong and Lamma, N. and N.E. winds, strong.
- 4.—South coast of China between Hongkong and Hailan, E. to N. winds, strong.

Shipping.

Arrivals.

Prinz-Regent Luitpold, Ger. s.s., 3,919, H. Kirchner, 6th Oct.—Bremen 25th Aug. and Singapore 1st Oct., Mails and Gen.—M. & Co.

Nils, Br. s.s., 4,779, E. P. Martin, R.N.R., 6th Oct.—Yokohama 21st Sept., Gen.—P. & O. S. N. Co.

Lian, Br. s.s., 1,357, C. O. Williams, 6th Oct.—Shanghai 3rd Oct., Gen.—B. & S.

Sungking, Br. s.s., 987, W. Shand, 6th Oct.—Cebu and Iloilo 1st Oct., Hemp and Copra.—B. & S.

Titan, Br. s.s., 5,720, R. Day, 6th Oct.—Tacoma via Ports 4th Sept., Flour, Lumber and Gen.—B. & S.

Daigai Maru, Jap. s.s., 864, H. Murayama, 6th Oct.—Swatow 5th Oct., Camphor and Gen.—O. & R.

Phraang, Ger. s.s., 1,011, Fr. von Manzel, 6th Oct.—Bangkok 27th Sept., Rice and Wood.—B. & S.

Footling, Br. s.s., 1,413, T. Lishman, 6th Oct.—Canton 5th Oct., Gen.—J. M. & Co.

Haitan, Br. s.s., 1,183, J. S. Roach, 6th Oct.—Swatow 5th Oct., Gen.—D. L. & Co.

Meefoo, Chi. s.s., 1,330, J. McArthur, 6th Oct.—Shanghai 3rd Oct., Gen.—O. M. S. N. Co.

Amara, Br. s.s., 1,561, Matlock, 6th Oct.—Hongkong 3rd Oct., Coal.—J. M. & Co.

Taikona Maru, Jap. s.s., 3,728, Fukui, 6th Oct.—Milke 30th Sept., Coal.—M. B. K.

Bloomfield, Br. s.s., 2,058, R. Linklater, 6th Oct.—New York 7th Sept., Gen.—S. T. & Co.

Opuck, Br. s.s., 2,517, R. J. Woodgat, 6th Oct.—Liverpool via Singapore 28th Sept., Gen.—B. & S.

Iechia, Ital. s.s., 2,481, G. Belsito, 6th Oct.—Haiphong 17th Sept., and Singapore 30th, Gen.—C. & Co.

Hasei, Fr. s.s., 742, J. Fannier, 6th Oct.—Haiphong via Pakhoi and Hailan 5th Oct., Gen.—A. R. M.

Clearance at the Harbour Office.

Amara, for Whampoa.

Mayon, for Canton.

Haitan, for Swatow.

Choyang, for Ningpo.

Quila, for Canton.

Tamir, for Manila.

Nila, for Singapore.

Taikona Maru, for Milke.

Bloomfield, for Swatow.

Bloomfield, for Shanghai.

Samur, for Bangkok.

Samur, for Canton.

Departures.

Oct. 6.

Prinz-Regent Luitpold, for Europe.

Drajer, for Swatow.

Meefoo, for Singapore.

Amara, for Colombo.

Hupah, for Canton.

Kueikow, for Tientsin.

Pangtung, for Bangkok.

Hongkong, for Coast Ports.

Samsen, for Bangkok.

Tamir, for Manila.

Japan, for Calcutta.

Choyang, for Shanghai.

Vine Branch, for Japan.

Suruga, for Singapore.

Prinz-Regent Luitpold, for Shanghai, &c.

Vessels arrived.

Per *Phanang*, from Bangkok—500 Chinese.

Per *Lian*, from Shanghai—Capt. Hall and Hutchison Messrs. Barber and Dohman.

Per *Nila*, from Yokohama—Messrs. Maitland, Master Miller, J. Lewis, Evans, Roy, Thomas, Mr. D. C. Brown, Miss Scott, Messrs. Glanville, Manning, Haldeman, Hillard, Mrs. Tweedie, nurse and infant, Mrs. Robinson, and Mr. and Mrs. Lachin.

Per *Prinz-Regent Luitpold*, for Hongkong from Southampton—Mr. W. T. Buckman, Rev. and Mrs. G. D. Thomson, Messrs. A. S. Wood, Mr. Sidney Losack, Geo. L. G. Chayne and C. H. Arnot. From Bremen—Messrs. B. Ida Müller and Ott. Johnson. From Antwerp—Mr. Th. Kerschjoggin. From Genoa—Messrs. Ed. Schöbber, H. Rapp, Miss Elisabeth Walter, Messrs. Max, Szelesinski, R. Kraft, F. Müller, K. Parker, H. Dinkels, Wong Koi and Hing Yung Kong. From Naples—Mr. Ed. Sacré. From Colombo—Messrs. C. H. Ford and A. Tie. From Penang—Messrs. J. Monal and Misses. From Singapore—Mr. and Mrs. C. Brockmann, Quak Yung Sim, C. F. Sutton, To Tee Seng, Moei Sien Sang, Mrs. Leong See, Mrs. Leong Ah Yew, Mr. and Mrs. Wong Seng Cheong, Messrs. Wong Cheong Tong, Lok Hok Chow, Mrs. M. Terabum, Messrs. R. Stan, and an infant.

Per *Opuck*, from Singapore—130 Chinese.

Per *Iechia*, from Singapore—Messrs. S. Greenstein, C. Elynski, and 185 Chinese.

Shipping Reports.

Str. *Haitan*, from Swatow—Strong N.E. breeze, rough sea, clear weather.

Str. *Meefoo*, from Shanghai—Strong N.E. monsoon in the Formosa Channel.

Str. *Iechia*, from Singapore—Squally weather and blowing hard from N.E.; no accident on the voyage.

VESSELS IN PORT.

Steamers.

Aki Maru, Jap. s.s., 3,095, K. Sato, 3rd Oct.—Shanghai 30th Sept., Coal, Pig Iron and Gen.—N. Y. K.

Ascania, Ger. s.s., 1,207, Classen, 4th Oct.—Samarang 21st Sept., Sugar.—H. A. L.

Bessie Dollar, Br. s.s., 2,797, A. Gow, 4th Oct.—Canton 3rd Oct., Gen.—A. K. & Co.

Bourbon, Fr. s.s., 950, Le Bail, 30th Sept.—Saigon 25th Sept., Rice.—Van Fat.

China, Am. s.s., 3,186, D. E. Friele, 1st Oct.—San Francisco 3rd Sept., Honolulu 10th, Yokohama 21st, Kobe 25th, Nagasaki 26th, and Shanghai 29th, Mails and Gen.—P. M. S. S. Co.

C. Ferd. Laeisz, Ger. s.s., 4,030, A. Wagner, 3rd Oct.—Shanghai 30th Sept., Gen.—H. A. L.

Hanyang, Br. s.s., 1,207, Trowbridge, 5th Oct.—China 28th Sept., Coal.—B. & S.

Harford, Br. s.s., 2,716, Pope, 21st Sept.—New York 30th June, and Labuan 15th Sept., Kerosine Oil.—S. O. Co.

Jacob Diederichsen, Ger. s.s., 630, A. Hansen, 5th Oct.—Haiphong 23rd Sept., and Hailan 3rd Oct., Gen.—J. & Co.

Kiang Ching, Chi. s.s., 1,002, A. F. Brisander, 3rd Oct.—Canton 2nd Oct., Gen.—King Lee & Co.

Kohschang, Ger. s.s., 1,297, O. Roselisky, 3rd Oct.—Bangkok 24th Sept., Teak Squares, Rice and Gen.—B. & S.

Kwelin, Br. s.s., 1,200, C. W. Puckett, 1st Oct.—Kauai 25th Sept., Coal.—B. & S.

Lightning, Br. s.s., 1,375, A. E. Gentles, 5th Oct.—Calcutta via Penang and Singapore 28th Sept., Gen.—D. S. & Co., Ltd.

Manchuria, Am. s.s., 8,750, A. Dixon, 5th Oct.—San Francisco 8th Sept., and Shanghai 3rd Oct., Mails and Gen.—P. M. S. S. Co.

Mausang, Br. s.s., 1,664, G. S. Welgall, 1st Oct.—Sandakan 25th Sept., Timber and Gen.—J. M. & Co.

Michael Jebson, Ger. s.s., 911, J. Petersen, 4th Oct.—Fagel 26th Sept., Sugar and Molasses.—J. & Co.

Oanfa, Br. s.s., 1,676, W. C. Lyett, 3rd Oct.—Manila 30th Sept., Gen.—B. & S.

Pheumphen, Br. s.s., 1,056, J. H. Scott, 26th Sept.—Saigon 22nd Sept., Rice and Gen.—Wo Fat Sing.

Prinz Sigismund, Ger. s.s., 3,300, D. Leoz, 21st Sept.—Sydney 26th Aug., and Manila 16th Sept., Gen.—M. & Co.

Proteus, Nor. s.s., 1,024, C. Müller, 4th Oct.—Bangkok 7th Sept., Rice.—Aagaard, Thorsen & Co.

Quinta, Ger. s.s., 987, F. Frahm, 4th Oct.—Sourabaya 24th Sept., Sugar.—J. O. J. L.

Rajah, Ger. s.s., 2,700, H. C. Reher, 28th Sept.—Bangkok and Kohschang 21st Sept., Teakwood and Rice.—B. & S.

Ryga, Nor. s.s., 2,492, Sørensen, 26th Sept.—from Probolinggo, Sugar.—P. & A. S. S. Co.

Wakamatsu Maru, Jap. s.s., 1,723, D. Aikawa, 2nd Oct.—Wakamatsu 27th Sept., Coal.—M. B. K.

Yuenang, Br. s.s., 1,128, P. H. Rolfe, 6th Oct.—Manila 1st Oct., Gen.—J. M. & Co.

Zafiro, Br. s.s., 1,629, R. Rodger, 5th Oct.—Manila 2nd Oct., Hemp and Gen.—S. T. & Co.

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VESSELS IN PORT.

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Aki Maru, Jap. s.s., 3,095, K. Sato, 3rd Oct.—Shanghai 30th Sept., Coal, Pig Iron and Gen.—N. Y. K.

Ascania, Ger. s.s., 1,207, Classen, 4th Oct.—Samarang 21st Sept., Sugar.—H. A. L.

Bessie Dollar, Br. s.s., 2,797, A. Gow, 4th Oct.—Canton 3rd Oct., Gen.—A. K. & Co.

Bourbon, Fr. s.s., 950, Le Bail, 30th Sept.—Saigon 25th Sept., Rice.—Van Fat.

China, Am. s.s., 3,186, D. E. Friele, 1st Oct.—San Francisco 3rd Sept., Honolulu 10th, Yokohama 21st, Kobe 25th, Nagasaki 26th, and Shanghai 29th, Mails and Gen.—P. M. S. S. Co.

C. Ferd. Laeisz, Ger. s.s., 4,030, A. Wagner, 3rd Oct.—Shanghai 30th Sept., Gen.—H. A. L.

Hanyang, Br. s.s., 1,207, Trowbridge, 5th Oct.—China 28th Sept., Coal.—B. & S.

Harford, Br. s.s., 2,716, Pope, 21st Sept.—New York 30th June, and Labuan 15th Sept., Kerosine Oil.—S. O. Co.

Jacob Diederichsen, Ger. s.s., 630, A. Hansen, 5th Oct.—Haiphong 23rd Sept., and Hailan 3rd Oct., Gen.—J. & Co.

Kiang Ching, Chi. s.s., 1,002, A. F. Brisander, 3rd Oct.—Canton 2nd Oct., Gen.—King Lee & Co.

Kohschang, Ger. s.s., 1,297, O. Roselisky, 3rd Oct.—Bangkok 24th Sept., Teak Squares, Rice and Gen.—B. & S.

Kwelin, Br. s.s., 1,200, C. W. Puckett, 1st Oct.—Kauai 25th Sept., Coal.—B. & S.

Lightning, Br. s.s., 1,375, A. E. Gentles, 5th Oct.—Calcutta via Penang and Singapore 28th Sept., Gen.—D. S. & Co., Ltd.

Manchuria, Am. s.s., 8,750, A. Dixon, 5th Oct.—San Francisco 8th Sept., and Shanghai 3rd Oct., Mails and Gen.—P. M. S. S. Co.

Mausang, Br. s.s., 1,664, G. S. Welgall, 1st Oct.—Sandakan 25th Sept., Timber and Gen.—J. M. & Co.

Michael Jebson, Ger. s.s., 911, J. Petersen, 4th Oct.—Fagel 26th Sept., Sugar and Molasses.—J. & Co.

Oanfa, Br. s.s., 1,676, W. C. Lyett, 3rd Oct.—Manila 30th Sept., Gen.—B. & S.

Pheumphen, Br. s.s., 1,056, J. H. Scott, 26th Sept.—Saigon 22nd Sept., Rice and Gen.—Wo Fat Sing.

Prinz Sigismund, Ger. s.s., 3,300, D. Leoz, 21st Sept.—Sydney 26th Aug., and Manila 16th Sept., Gen.—M. & Co.

Proteus, Nor. s.s., 1,024, C. Müller, 4th Oct.—Bangkok 7th Sept., Rice.—Aagaard, Thorsen & Co.

Quinta, Ger. s.s., 987, F. Frahm, 4th Oct.—Sourabaya 24th Sept., Sugar.—J. O. J. L.

Rajah, Ger. s.s., 2,700, H. C. Reher, 28th Sept.—Bangkok and Kohschang 21st Sept., Teakwood and Rice.—B. & S.

Ryga, Nor. s.s., 2,492, Sørensen, 26th Sept.—from Probolinggo, Sugar.—P. & A. S. S. Co.

Wakamatsu Maru, Jap. s.s., 1,723, D. Aikawa, 2nd Oct.—Wakamatsu 27th Sept., Coal.—M. B. K.

Yuenang, Br. s.s., 1,128, P. H. Rolfe, 6th Oct.—Manila 1st Oct., Gen.—J. M. & Co.

Zafiro, Br. s.s., 1,629, R. Rodger, 5th Oct.—Manila 2nd Oct., Hemp and Gen.—S. T. & Co.

Shipping Reports.

Str. *Haitan*, from Swatow—Strong N.E. breeze, rough sea, clear weather.

Str. *Meefoo*, from Shanghai—Strong N.E. monsoon in the Formosa Channel.

Str. *Iechia*, from Singapore—Squally weather and blowing hard from N.E.; no accident on the voyage.

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Estimations

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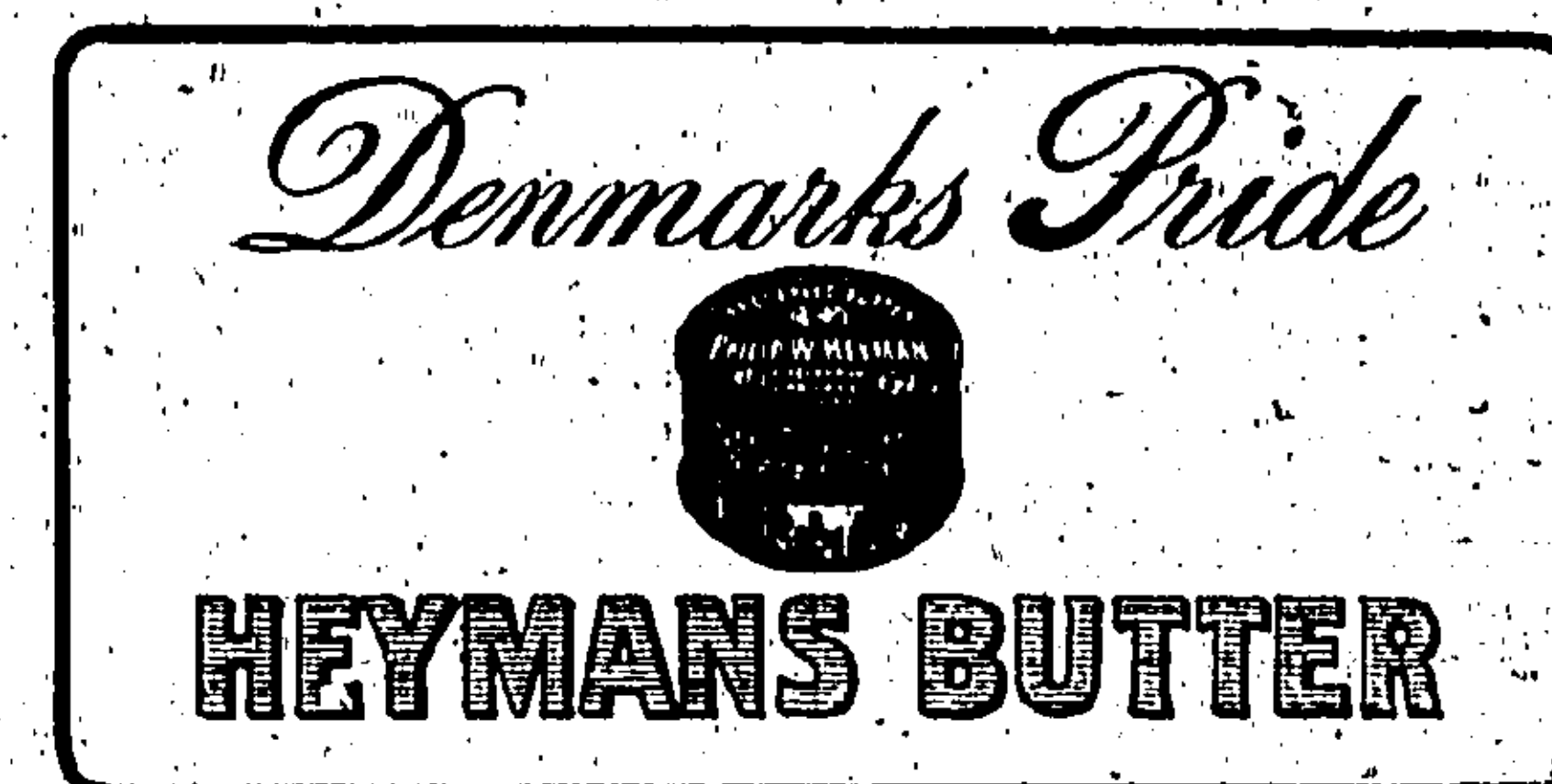
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nent injury, or other impairment of the blood from whatever cause it may arise. No sooner is it introduced into the
system than it permeates and purifies to the minutest capillaries, overcoming and expelling the virus of disease,
wherever and in whatsoever form met with; removing all blotches, pimples, scurs, scurvy, scrofulous and
glandular swellings, discolorations, roughness and unsightly patches, &c. Its effects are almost magical in the
treatment of gonorrhea, rheumatism, sciatica, lumbago, pains and swellings of the joints, discharges, secondary
syphilis, eruptions, skin diseases, itching humors, hemorrhoids, piles, fistulas, ulcers, &c. It cures all venereal
symptoms, eczema, lepra, psoriasis, bad legs, bad breasts, abscesses, carbuncles, erysipelas, furunculosis, freckles,
strains the venous system, restores the circulation, standing brachitis, asthma, and heaving, straining
rheumoid and dropsical feet, often the precursor of consumption.

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THE LATEST NEWS

of the

11. OFFICIAL BOARD GENERAL

From the University of Penn's

Hongkong, 16th April, 1905.

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By Order

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Houghton & Phipps Co., Ltd.
May 1909, and December 1908.

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